



COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION DIVISION OF PLANNING

IMPROVEMENTS AT LYNN FELLS PARKWAY AND MELROSE STREET IN THE CITY OF MELROSE MASSACHUSETTS MIDDLESEX COUNTY

THESE PLANS ARE SUPPLEMENTED BY THE OCTOBER 2017 CONSTRUCTION STANDARD DETAILS, THE 2015 OVERHEAD SIGNAL STRUCTURE AND FOUNDATION STANDARD DRAWINGS, MASSDOT TRAFFIC MANAGEMENT PLANS AND DETAIL DRAWINGS, THE 1990 STANDARD DRAWINGS FOR SIGNS AND SUPPORTS, THE 1968 STANDARD DRAWINGS FOR TRAFFIC SIGNALS AND HIGHWAY LIGHTING, AND THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK.

DCR CONTRACT NO. P20-3361-D1A

NOTICE OF INTENT SUBMISSION

MAURA HEALEY, GOVERNOR

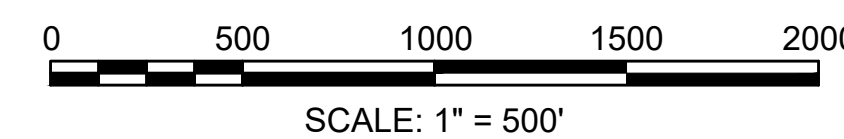
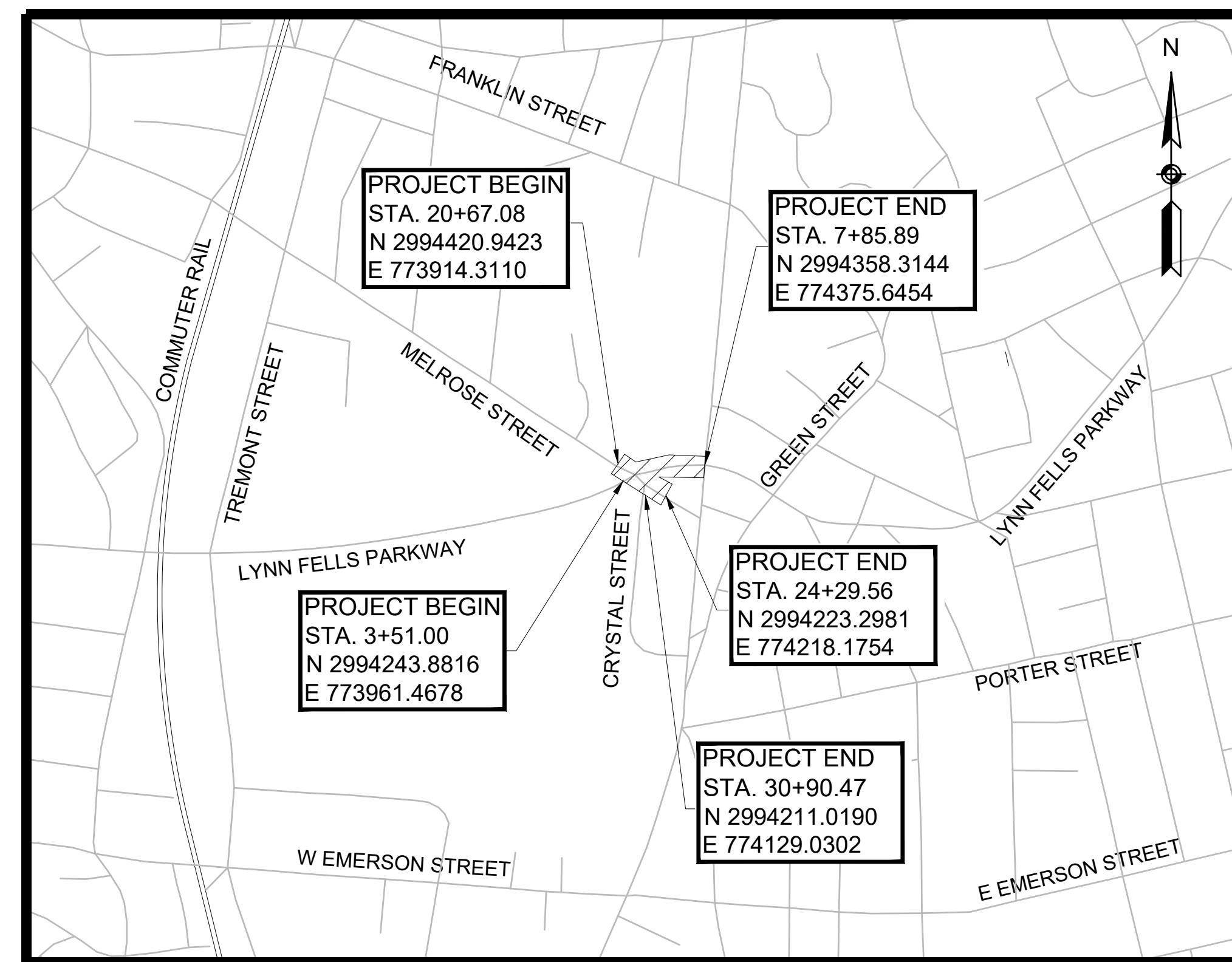
KIM DRISCOLL, LT. GOVERNOR

REBECCA TEPPER, SECRETARY,
EXECUTIVE OFFICE OF ENERGY & ENVIRONMENTAL AFFAIRS

DOUG RICE, COMMISSIONER
DEPARTMENT OF CONSERVATION & RECREATION

ROBERT LOWELL, ACTING CHIEF ENGINEER
DEPARTMENT OF CONSERVATION & RECREATION

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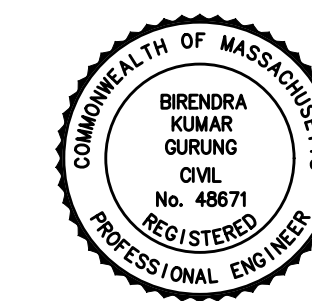


LENGTH OF PROJECT = 797.37 FEET = 0.1510 MILES

DESIGN DESIGNATION (LYNN FELLS PARKWAY)

DESIGN SPEED	25 MPH
ADT (2020)	14,130
ADT (2030)	14,853
K	7.8%
D	55% EB
T (PEAK HOUR)	3.1%
T (AVERAGE DAY)	3.6%
DHV	1,160
DDHV	702
FUNCTIONAL CLASSIFICATION	URBAN MINOR ARTERIAL

TO THE BEST OF MY KNOWLEDGE, INFORMATION, AND BELIEF, I CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN CONFORMANCE WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS.



8/16/2023

BIRENDRA GURUNG, PE, PTOE, ENV SP, LEED GREEN ASSOCIATE
MASSACHUSETTS REG. NO.
REGISTERED PROFESSIONAL ENGINEER

REV.	DATE	DESCRIPTION	BY
1	4/11/2023	25% DESIGN SUBMISSION	

Nitsch Engineering
2 Center Plaza, Suite 430
Boston, MA 02108
T: (617) 338-0063
F: (617) 338-0472

- Civil Engineering
- Land Surveying
- Transportation Engineering
- Structural Engineering
- Green Infrastructure
- Planning
- GIS

COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION

INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

SURVEY BY: NITSCH	TITLE SHEET & INDEX	SHEET NO.
DRAWN BY: KC		1
CHECKED BY: BG	CONT: P20-3361-D1A	SCALE: AS NOTED
APPROVED BY: JM	DATE: 4/11/2023	1 OF 33

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ABBREVIATIONS

Table with 2 columns: GENERAL and ABBREVIATIONS. Lists various engineering terms like AADT, ABAN, ADJ, APPROX., A.C., ACCM PIPE, BIT., BC, BD., BL, BLDG, BM, BO, BOS, BR., CB, CBCI, CC, CCM, CEM, CI, CIP, CLF, CL, CMP, CSP, CO., CONC, CONT, CONST, CR GR, DHV, DI, DIA, DIP, DW, DWY, ELEV (or EL.), EMB, EOP, EXIST (or EX), EXC, F&C, F&G, FDN., FLDSTN, GAR, GD, GG, GI, GIP, GRAN, GRAV, GRD, HDW, HMA, HOR, HYD, INV, JCT, L, LB, LP, LT, MAX, MB, MH, MHB, MIN, NIC, NO., PC, PCC, P.G.L., PI, POC, POT, PRC, PROJ, PROP, PSB, PT, PVC, PVI, PVT, PVM, PWW.

ABBREVIATIONS (cont.)

Table with 2 columns: GENERAL and ABBREVIATIONS. Lists terms like R, R&D, RCP, RD, RDWY, REM, RET, RET WALL, ROW, RR, R&R, R&S, RT, SB, SHLD, SHLD, ST, STA, SSD, SHLO, SW, T, TAN, TEMP, TC, TOS, TYP, UP, VAR, VERT, VC, WCR, WG, WIP, WM, X-SECT, NTS.

TRAFFIC SIGNAL ABBREVIATIONS

Table with 2 columns: ABBREVIATIONS and DESCRIPTION. Lists terms like CAB, CCVE, DW, FDW, FR, FRL, FRR, FY, FYL, FYR, G, GL, GR, GSL, GSR, GV, OL, PED, PTZ, R, RL, RR, TR SIG, TSC, W, Y, YL.

GENERAL SYMBOLS

Table with 3 columns: EXISTING, PROPOSED, and DESCRIPTION. Lists symbols for various features like JB, CB, FP, GP, MB, WELL, EHH, GG, BHL #, MW #, TP #, CO.BD., MHB, MON, SB, TB, TPL or GUY, UFB, UPDL, ULT, UPL, BUSH, TREE, STUMP, SWAMP / MARSH, WATER GATE, PARKING METER, OVERHEAD CABLE/WIRE, CURBING, CONTOURS, UNDERGROUND DRAIN PIPE, UNDERGROUND ELECTRIC DUCT, UNDERGROUND GAS MAIN, UNDERGROUND SEWER MAIN, UNDERGROUND TELEPHONE DUCT, UNDERGROUND WATER MAIN, BALANCED STONE WALL, GUARD RAIL - STEEL POSTS, GUARD RAIL - WOOD POSTS, GUARD RAIL - DOUBLE FACE - STEEL POSTS, GUARD RAIL - DOUBLE FACE - WOOD POSTS, CHAIN LINK OR METAL FENCE, WOOD FENCE, HAY BALES/SILT FENCE, TREE LINE, SAWCUT LINE, TOP OR BOTTOM OF SLOPE, LIMIT OF EDGE OF PAVEMENT OR COLD PLANE AND OVERLAY, BANK OF RIVER OR STREAM, BORDER OF WETLAND, 100 FT WETLAND BUFFER, 200 FT RIVERFRONT BUFFER, STATE HIGHWAY LAYOUT, TOWN OR CITY LAYOUT, COUNTY LAYOUT, RAILROAD SIDELINE, TOWN OR CITY BOUNDARY LINE, PROPERTY LINE OR APPROXIMATE PROPERTY LINE, EASEMENT.

TRAFFIC SYMBOLS

Table with 3 columns: EXISTING, PROPOSED, and DESCRIPTION. Lists symbols for traffic control elements like CONTROLLER PHASE ACTUATED, TRAFFIC SIGNAL HEAD, WIRE LOOP DETECTOR, VIDEO DETECTION CAMERA, MICROWAVE DETECTOR, PEDESTRIAN PUSH BUTTON, EMERGENCY PREEMPTION CONFIRMATION STROBE LIGHT, VEHICULAR SIGNAL HEAD, VEHICULAR SIGNAL HEAD, OPTICALLY PROGRAMMED, FLASHING BEACON, PEDESTRIAN SIGNAL HEAD, RAILROAD SIGNAL, SIGNAL POST AND BASE, MAST ARM, SIGN AND POST, SIGN AND POST (2 POSTS), MAST ARM WITH LUMINAIRE, OPTICAL PRE-EMPTION DETECTOR, CONTROL CABINET, FLASHING BEACON CONTROL AND METER PEDESTAL, LOAD CENTER ASSEMBLY, PULL BOX, ELECTRIC HANDHOLE, TRAFFIC SIGNAL CONDUIT.

PAVEMENT MARKINGS SYMBOLS

Table with 3 columns: EXISTING, PROPOSED, and DESCRIPTION. Lists symbols for pavement markings like PAVEMENT ARROW - WHITE, LEGEND "ONLY" - WHITE, STOP LINE - 12" WIDE, CROSSWALK - 10" WIDE, SOLID WHITE LINE - 6" WIDE, SOLID YELLOW LINE - 6" WIDE, BROKEN WHITE LINE, BROKEN YELLOW LINE, DOTTED WHITE LINE, DOTTED YELLOW LINE, DOTTED WHITE LINE EXTENSION, DOTTED YELLOW LINE EXTENSION, DOUBLE WHITE LINE, DOUBLE YELLOW LINE.

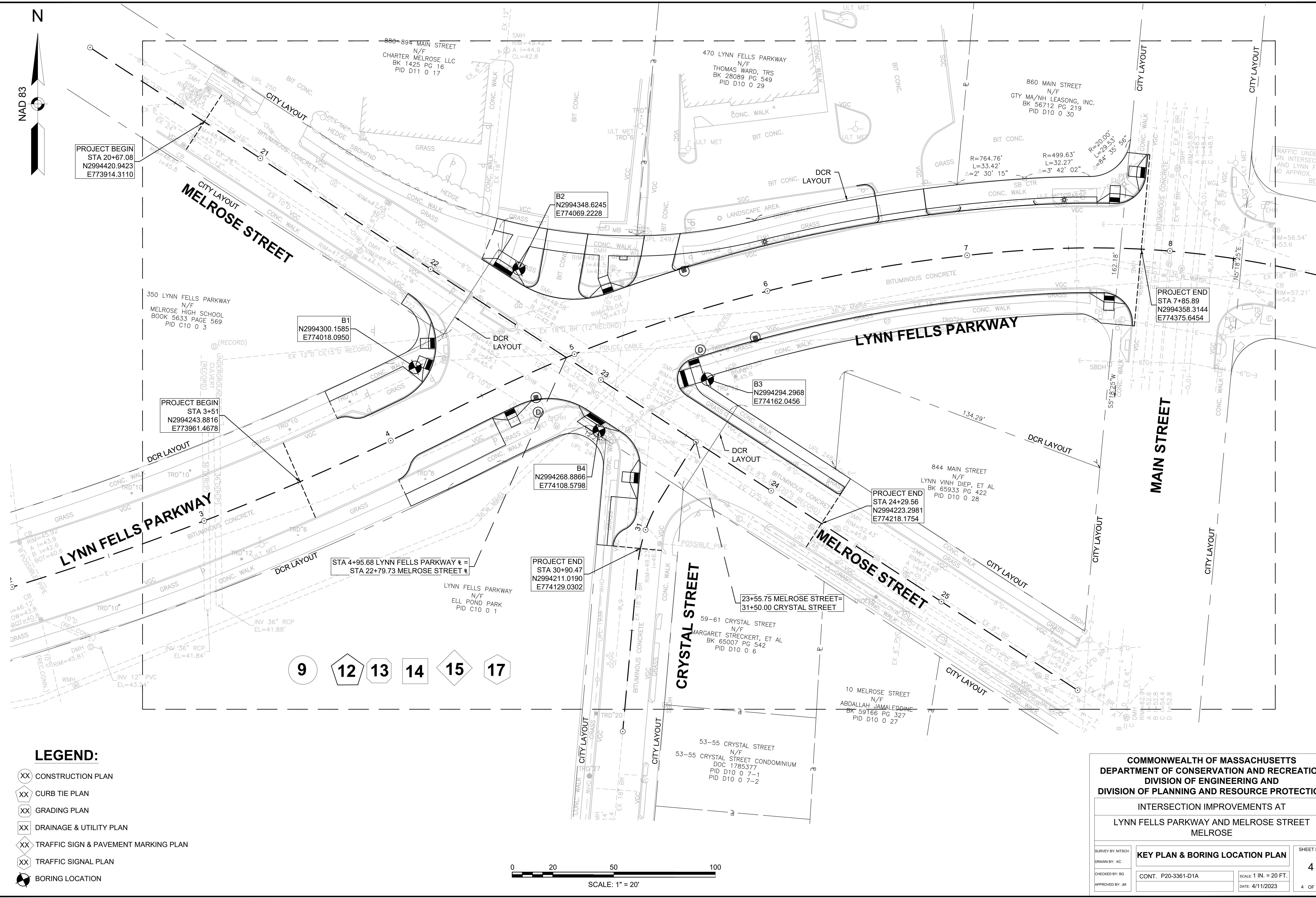
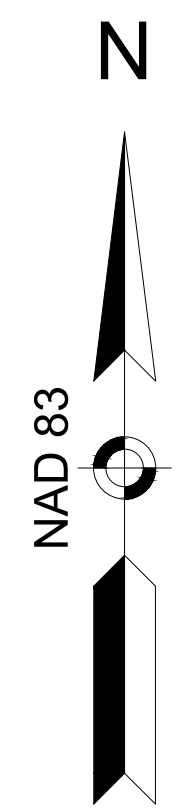
COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION DIVISION OF ENGINEERING AND DIVISION OF PLANNING AND RESOURCE PROTECTION. INTERSECTION IMPROVEMENTS AT LYNN FELLS PARKWAY AND MELROSE STREET MELROSE. LEGEND & ABBREVIATIONS. SHEET NO. 2 OF 33. DATE: 4/11/2023.

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GENERAL NOTES

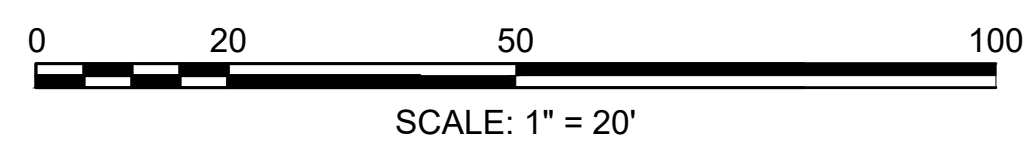
1. THE EXISTING TOPOGRAPHIC CONDITIONS SHOWN ON THESE PLANS CONSIST OF ON-THE-GROUND INSTRUMENT SURVEY PERFORMED BY NITSCH ENGINEERING IN JANUARY OF 2020.
2. COORDINATES ARE PROVIDED IN US SURVEY FEET, REFERENCED TO THE NORTH AMERICAN DATUM OF 1983 (NAD83). ELEVATIONS ARE PROVIDED IN US SURVEY FEET, REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
3. THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY EXISTING GRADES AND ELEVATIONS AT THE LOCATIONS WHERE PROPOSED WORK MEETS EXISTING CONDITIONS.
4. THE CONTRACTOR SHALL BE RESPONSIBLE TO MAKE HIS OWN DETERMINATION OF SUBSURFACE CONDITIONS INCLUDING THE LOCATION OF ROCK AND THE ACTUAL LOCATION OF UTILITIES OR OTHER FEATURES WHICH MAY AFFECT HIS WORK.
5. EXISTING UTILITIES SHOWN ON THESE PLANS WERE COMPILED FROM FIELD SURVEYS AND VARIOUS OTHER SOURCES. LOCATIONS ARE NOT GUARANTEED TO BE ACCURATE NOR IS IT GUARANTEED THAT ALL UTILITIES ARE SHOWN. NO SEPARATE OR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR DUE TO ANY VARIANCE BETWEEN THE DATA SHOWN ON THE PLANS AND ACTUAL FIELD CONDITIONS ENCOUNTERED. WHERE AN EXISTING UTILITY IS FOUND TO CONFLICT WITH THE PROPOSED WORK, THE LOCATION, ELEVATION AND SIZE OF THE UTILITY SHALL BE ACCURATELY DETERMINED WITHOUT DELAY BY THE CONTRACTOR AND THIS INFORMATION FURNISHED TO THE ENGINEER.
6. THE RELOCATION, INSTALLATION OR REMOVAL OF PRIVATE UTILITIES SHALL BE ACCOMPLISHED BY THEIR OWNERS, EXCEPT AS OTHERWISE NOTED. THE CONTRACTOR WILL BE REQUIRED TO COOPERATE WITH THE PRIVATE UTILITY COMPANIES AND ALLOW THEM ADEQUATE TIME TO COMPLETE THEIR WORK IN ADVANCE OF PERFORMING ANY PAVING OPERATIONS OR OTHER FINISHED WORK.
7. AREAS OUTSIDE OF THE LIMITS OF PROPOSED WORK DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION AND TO THE SATISFACTION OF THE ENGINEER.
8. THE CONTRACTOR SHALL CONTACT "DIG SAFE" AT 1-888-DIG-SAFE AT LEAST 72 HOURS PRIOR TO COMMENCING WORK ON THE PROJECT.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND FOR MAINTAINING COMPOST FILTER TUBES, SILT FENCE(S), AND OTHER EROSION CONTROL MEASURES THROUGHOUT THE DURATION OF THE CONTRACT AS SHOWN ON THE PLANS AND AS REQUIRED BY THE ENGINEER.
10. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL SAFETY CODES AND LEGAL REQUIREMENTS, IN THE CONSTRUCTION OF IMPROVEMENTS.
11. UNLESS OTHERWISE NOTED OR APPROVED BY THE ENGINEER, THE CONTRACTOR SHALL MAINTAIN ALL EXISTING UTILITIES IN SERVICE AT ALL TIMES. IF THE CONTRACTOR DAMAGES UTILITY SYSTEMS, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESPECTIVE UTILITY COMPANY AND SHALL REPAIR/REPLACE THE AFFECTED SYSTEM AT HIS OWN EXPENSE.
12. ALL MATERIALS TO BE REMOVED AND DISCARDED SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL APPLICABLE CODES AND REGULATIONS.
13. THE TEMPORARY TRAFFIC CONTROL PLANS INDICATE THE GENERAL REQUIREMENTS FOR THE VARIOUS PHASES OF WORK. THE CONTRACTOR SHALL SUBMIT DETAILED TRAFFIC MANAGEMENT PLANS TO THE ENGINEER FOR APPROVAL.
14. THE FLOW OF TRAFFIC THROUGH THE SITE MUST BE MAINTAINED AS SHOWN ON THE TRAFFIC CONTROL PLANS AND SPECIFIED IN THE SPECIAL PROVISIONS. CONSTRUCTION EQUIPMENT AND MATERIALS SHALL NOT BE PARKED OR STOCKPILED SO AS TO OBSTRUCT THE FLOW OF VEHICLES.
15. THE TERM "PROPOSED" (PROP) MEANS WORK TO BE CONSTRUCTED USING NEW MATERIALS OR, WHERE APPLICABLE, RE-USING EXISTING MATERIALS IN SUITABLE CONDITION IDENTIFIED AS "REMOVE AND RESET" (R&R).
16. SURFACE JOINTS BETWEEN NEW HOT MIX ASPHALT ROADWAY PAVEMENT AND SAWCUT EXISTING PAVEMENT SHALL BE SEALED WITH HMA JOINT SEALANT.
17. ALL EXISTING GRANITE CURB & EDGING IN SUITABLE CONDITION SHALL BE RE-USED IN THE PROPOSED WORK, EXCEPT CURVED STONES OF A DIFFERENT RADIUS THAN PROPOSED CURB.
18. IN AREAS OF SIDEWALK RECONSTRUCTION WHERE PROPOSED MEETS EXISTING PAVEMENT, THE EXISTING PAVEMENT SHALL BE SAWCUT TO OBTAIN A CLEAN VERTICAL FACE.
19. THE LAYOUT OF ALL NEW PEDESTRIAN RAMPS SHALL CONFORM TO ADA/AAB STANDARDS AND CURRENT MASSDOT STANDARDS AND DIRECTIVES.
20. THE CONTRACTOR WILL BE REQUIRED TO SET SURVEY CONTROL USING A PROFESSIONAL LAND SURVEYOR (PLS) AND COORDINATE THE SURVEY WORK WITH THE DEPARTMENT OF CONSERVATION AND RECREATION (DCR).
21. THE CONTRACTOR SHALL PROVIDE INLET PROTECTION DURING CONSTRUCTION FOR ALL EXISTING AND PROPOSED CATCH BASINS/AREA DRAINS WITHIN THE LIMIT OF WORK. REFER TO THE SPECIAL PROVISIONS AND THE DETAIL ENTITLED "CATCH BASIN W/ SILT SACK INLET PROTECTIONS" ON THE CONSTRUCTION DETAIL SHEETS.

COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION DIVISION OF ENGINEERING AND DIVISION OF PLANNING AND RESOURCE PROTECTION			
INTERSECTION IMPROVEMENTS AT LYNN FELLS PARKWAY AND MELROSE STREET MELROSE			
SURVEY BY: NITSCH	GENERAL NOTES		SHEET NO.
DRAWN BY: KC			3
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APPROVED BY: JM		DATE: 4/11/2023	3 OF 33



LEGEND:

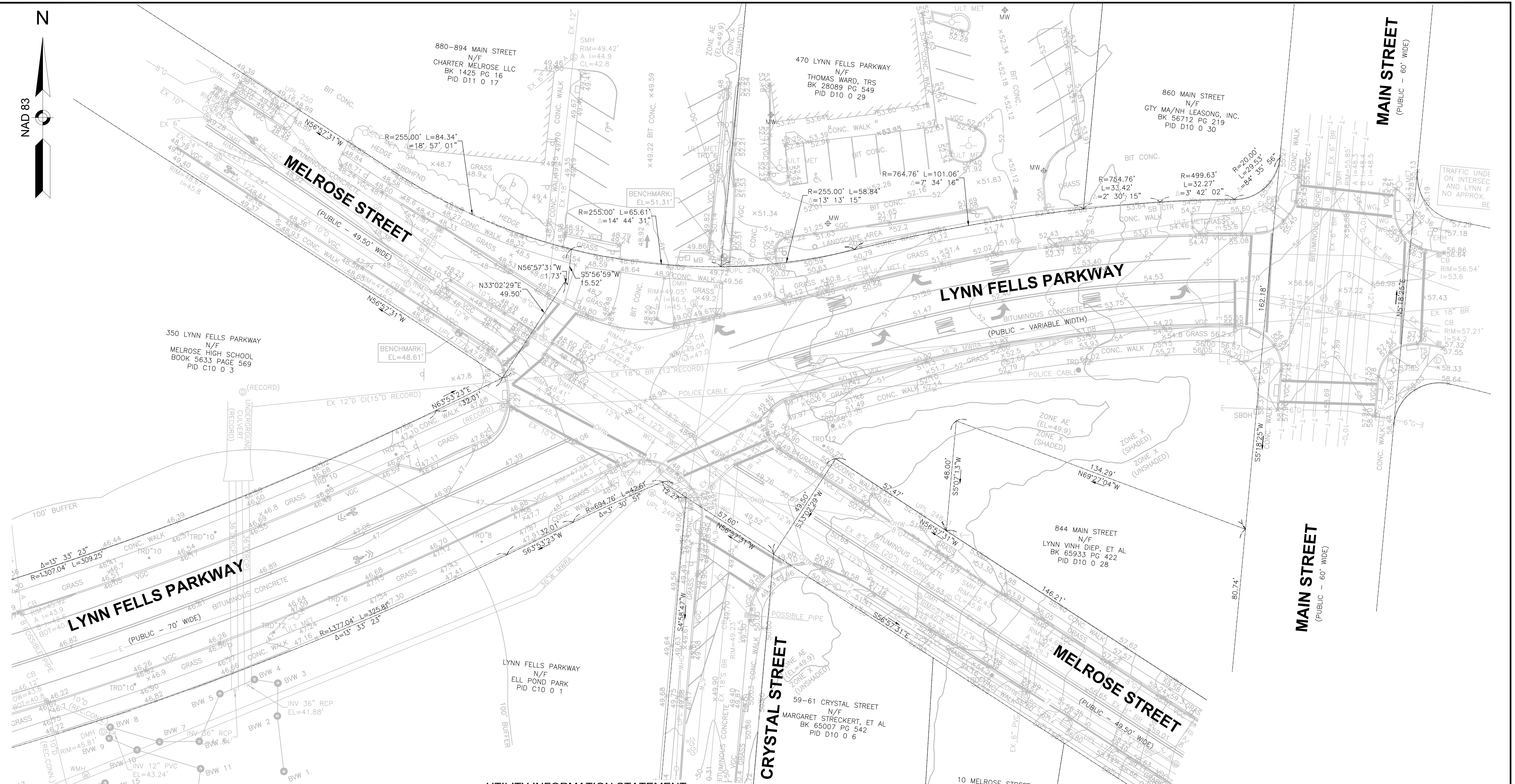
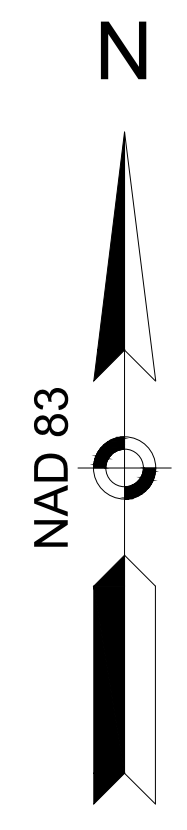
- XX CONSTRUCTION PLAN
- XX CURB TIE PLAN
- XX GRADING PLAN
- XX DRAINAGE & UTILITY PLAN
- XX TRAFFIC SIGN & PAVEMENT MARKING PLAN
- XX TRAFFIC SIGNAL PLAN
- ⊙ BORING LOCATION



**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION**

INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

SURVEY BY: NITSCH	KEY PLAN & BORING LOCATION PLAN	SHEET NO. 4
DRAWN BY: KC		
CHECKED BY: BG		
APPROVED BY: JM	CONT. P20-3361-D1A	SCALE: 1 IN. = 20 FT. DATE: 4/11/2023
		4 OF 33

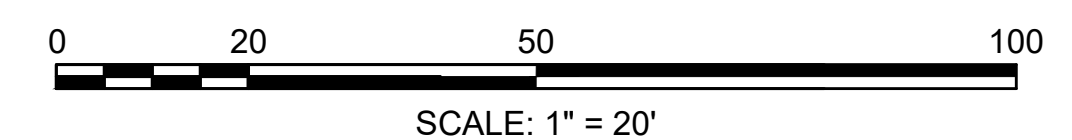


NOTES

1. THIS DOCUMENT IS AN INSTRUMENT OF SERVICE OF NITSCH ENGINEERING. IT IS ISSUED TO THE MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION (DCR) FOR PURPOSES RELATED DIRECTLY AND SOLELY TO NITSCH ENGINEERING'S SCOPE OF SERVICES UNDER CONTRACT WITH DCR FOR LAND SURVEYING SERVICES. ANY USE OR REUSE OF THIS DOCUMENT FOR ANY REASON BY ANY PARTY FOR PURPOSES UNRELATED DIRECTLY AND SOLELY TO SAID CONTRACT AND PROJECT SHALL BE AT THE USER'S SOLE AND EXCLUSIVE RISK AND LIABILITY, INCLUDING LIABILITY FOR VIOLATION OF COPYRIGHT LAWS, UNLESS WRITTEN AUTHORIZATION IS GIVEN THEREFOR BY NITSCH ENGINEERING.
2. THE PURPOSE OF THIS PLAN IS TO SHOW EXISTING CONDITIONS AS THE RESULT OF AN ON-THE-GROUND INSTRUMENT SURVEY WHICH WAS COMPLETED ON JANUARY 18, 2020.
3. HORIZONTAL COORDINATES REFER TO MASSACHUSETTS STATE PLANE COORDINATE SYSTEM (NAD83) BASED ON GPS OBSERVATIONS.
4. ELEVATION REFERS TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAV88) BASED ON GPS OBSERVATIONS.
5. THE WETLANDS FLAGS SHOWN HEREON ARE AS DELINEATED BY LEC ENVIRONMENTAL CONSULTANTS, INC. ON JANUARY 15, 2020.
6. THE FEMA FLOOD ZONES SHOWN HERE ON ARE AS SHOWN AS GRAPHICALLY DEPICTED ON FLOOD INSURANCE RATE MAPS 25017C0429E AND 25017C0433E AND THE FEMA LETTER OF MAP REVISION DATED DECEMBER 3, 2018.
7. THE INFORMATION CONTAINED ON THE DISK OR ELECTRONIC DRAWING FILE ACCOMPANYING THIS PLAN MUST BE COMPARED TO THE SEALED AND SIGNED HARD COPY OF THE PLAN TO ENSURE THE ACCURACY OF ALL INFORMATION AND TO ENSURE NO CHANGES, ALTERATIONS, OR MODIFICATIONS HAVE BEEN MADE. RELIANCE SHALL NOT BE MADE ON A DOCUMENT TRANSMITTED BY COMPUTER OR OTHER ELECTRONIC MEANS UNLESS FIRST COMPARED TO THE ORIGINAL SEALED DOCUMENT ISSUED AT THE TIME OF THE SURVEY. DUE TO THE CRITICAL NATURE OF SURVEYING, DATA ACQUISITION, AND AUTOCAD PLAN DEVELOPMENT, IF CRITICAL DIMENSIONAL INFORMATION IS NEEDED AND IS NOT SPECIFICALLY SHOWN ON THE ELECTRONIC DRAWING FILE, PLEASE CONTACT NITSCH ENGINEERING.

UTILITY INFORMATION STATEMENT

1. THE SUB-SURFACE UTILITY INFORMATION SHOWN HEREON IS COMPILED BASED ON FIELD SURVEY INFORMATION, RECORD INFORMATION AS SUPPLIED BY THE APPROPRIATE UTILITY COMPANIES, AND PLAN INFORMATION SUPPLIED BY THE CLIENT, IF ANY; THEREFORE WE CANNOT GUARANTEE THE ACCURACY OF SAID COMPILED SUB-SURFACE INFORMATION TO ANY CERTAIN DEGREE OF STATED TOLERANCE. ONLY PHYSICALLY LOCATED SUB-SURFACE UTILITY FEATURES FALL WITHIN NORMAL STANDARD OF CARE ACCURACIES.
2. THE LOCATIONS OF UNDERGROUND PIPES, CONDUITS, AND STRUCTURES HAVE BEEN DETERMINED FROM SAID INFORMATION, AND ARE APPROXIMATE ONLY. COMPILED LOCATIONS OF ANY UNDERGROUND STRUCTURES, NOT VISIBLY OBSERVED AND LOCATED, CAN VARY FROM THEIR ACTUAL LOCATIONS.
3. ADDITIONAL BURIED UTILITIES/STRUCTURES MAY BE ENCOUNTERED.
4. THE STATUS OF UTILITIES, WHETHER ACTIVE, ABANDONED, OR REMOVED, IS AN UNKNOWN CONDITION AS FAR AS OUR COMPILATION OF THIS INFORMATION.
5. IT IS INCUMBENT UPON INDIVIDUALS USING THIS INFORMATION TO UNDERSTAND THAT COMPILING UTILITY INFORMATION IS NOT EXACT, AND IS SUBJECT TO CHANGE BASED UPON VARYING PLAN INFORMATION RECEIVED AND ACTUAL LOCATIONS.
6. THE ACCURACY OF MEASURED UTILITY INVERTS AND PIPE SIZES IS SUBJECT TO FIELD CONDITIONS, THE ABILITY TO MAKE VISUAL OBSERVATIONS, DIRECT ACCESS TO THE VARIOUS ELEMENTS AND OTHER MATTERS.
7. THE PROPER UTILITY ENGINEERING/COMPANY SHOULD BE CONSULTED AND THE ACTUAL LOCATIONS OF SUBSURFACE STRUCTURES SHOULD BE VERIFIED IN THE FIELD (V.I.F.) BEFORE PLANNING FUTURE CONNECTIONS. CONTACT THE DIG SAFE CALL CENTER AT 1-888-344-7233, SEVENTY-TWO HOURS PRIOR TO EXCAVATION, BLASTING, GRADING, AND/OR PAVING.
8. AS OF THE DATE OF THIS PLAN RECORD INFORMATION HAS NOT BEEN RECEIVED BY NITSCH ENGINEERING FOR THE FOLLOWING UTILITIES: TELEPHONE (VERIZON)



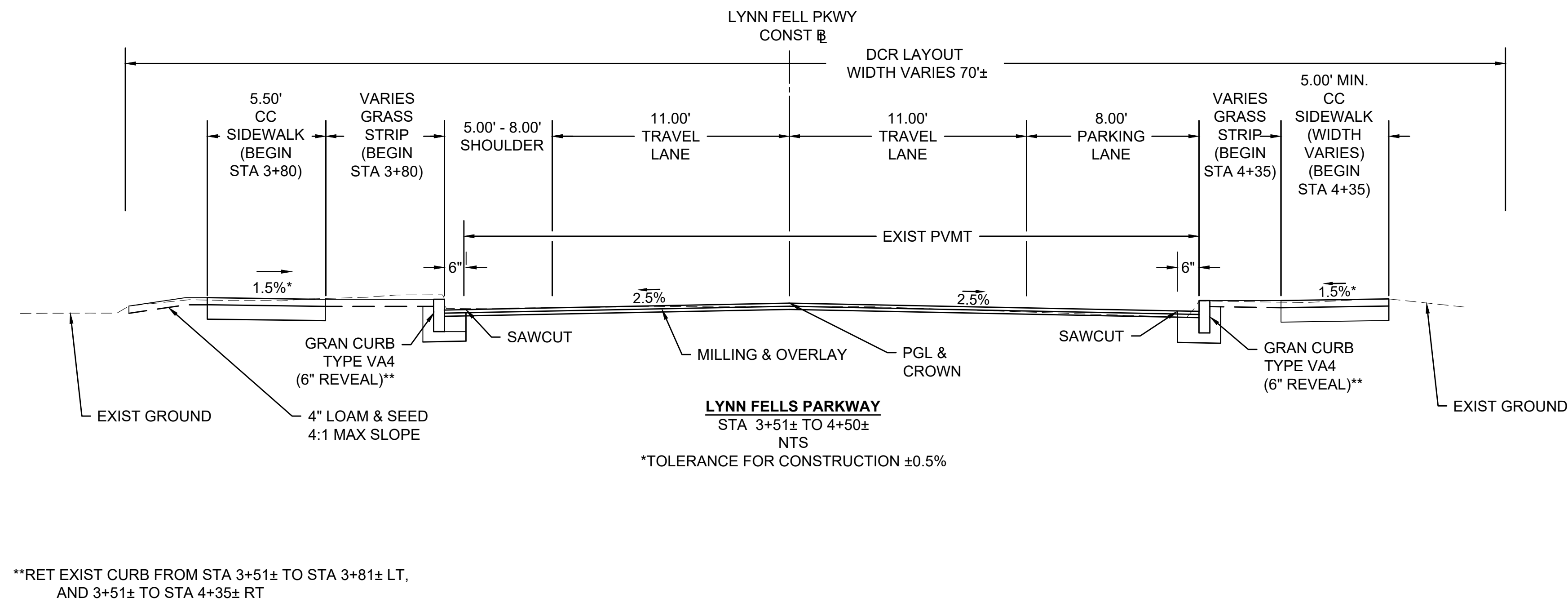
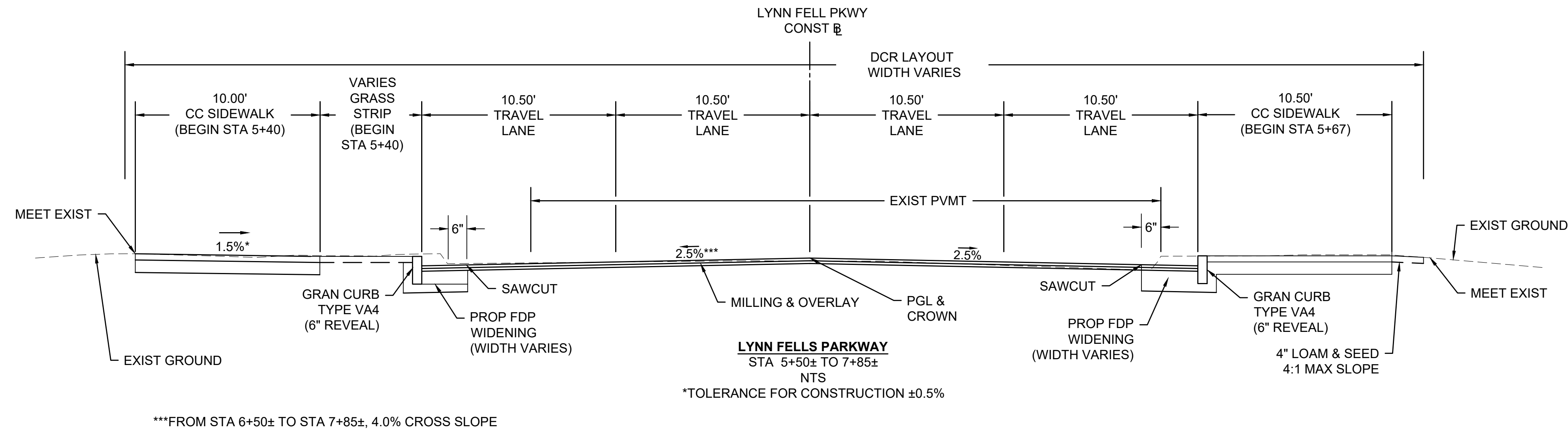
**COMMONWEALTH OF MASSACHUSETTS
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**INTERSECTION IMPROVEMENTS AT
LYNN FELS PARKWAY AND MELROSE STREET
MELROSE**

SURVEY BY: NITSCH	EXISTING CONDITIONS PLAN	SHEET NO. 5
DRAWN BY: KC		
CHECKED BY: BG		
APPROVED BY: JM	CONT. P20-3361-D1A	SCALE: 1 IN. = 20 FT. DATE: 4/11/2023
		5 OF 33

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PAVEMENT NOTES:

PROPOSED STANDARD MILLING & OVERLAY

SURFACE: 1.75" SUPERPAVE SURFACE COURSE - 12.5 (SSC-12.5) OVER
1.75" SUPERPAVE INTERMEDIATE COURSE - 12.5 (SIC-12.5) OVER
MILLING: VARIABLE DEPTH PAVEMENT STANDARD MILLING (3.5" MAX)

PROPOSED FULL DEPTH PAVEMENT

SURFACE: 1.75" SUPERPAVE SURFACE COURSE - 12.5 (SSC-12.5) OVER
1.75" SUPERPAVE INTERMEDIATE COURSE - 12.5 (SIC-12.5)
BASE: 4.00" SUPERPAVE BASE COURSE - 37.5 (SBC-37.5)
SUBBASE: 4" DENSE GRADED CRUSHED STONE FOR SUBBASE OVER
8" GRAVEL BORROW, TYPE B

PROPOSED FULL DEPTH PAVEMENT WIDENING LESS THAN 4 FEET

SURFACE: 1.75" SUPERPAVE SURFACE COURSE - 12.5 (SSC-12.5) OVER
1.75" SUPERPAVE INTERMEDIATE COURSE - 12.5 (SIC-12.5)
BASE: 6" HIGH EARLY STRENGTH CEMENT CONCRETE
SUBBASE: 8" GRAVEL BORROW, TYPE B

PROPOSED CEMENT CONCRETE SIDEWALK / PEDESTRIAN CURB RAMP

SURFACE: 4" CEMENT CONCRETE SIDEWALK (4000 PSI, 3/4", 610)
SUBBASE: 8" GRAVEL BORROW, TYPE B

PROPOSED CEMENT CONCRETE SIDEWALK AT DRIVEWAY

SURFACE: 6" CEMENT CONCRETE SIDEWALK (4000 PSI, 3/4", 610)
SUBBASE: 8" GRAVEL BORROW, TYPE B

PROPOSED HOT MIX ASPHALT DRIVEWAY

SURFACE: 1.75" SUPERPAVE SURFACE COURSE - 12.5 (SSC-12.5) OVER
3" SUPERPAVE INTERMEDIATE COURSE - 12.5 (SIC-12.5)
SUBBASE: 8" GRAVEL BORROW, TYPE B

NOTES:

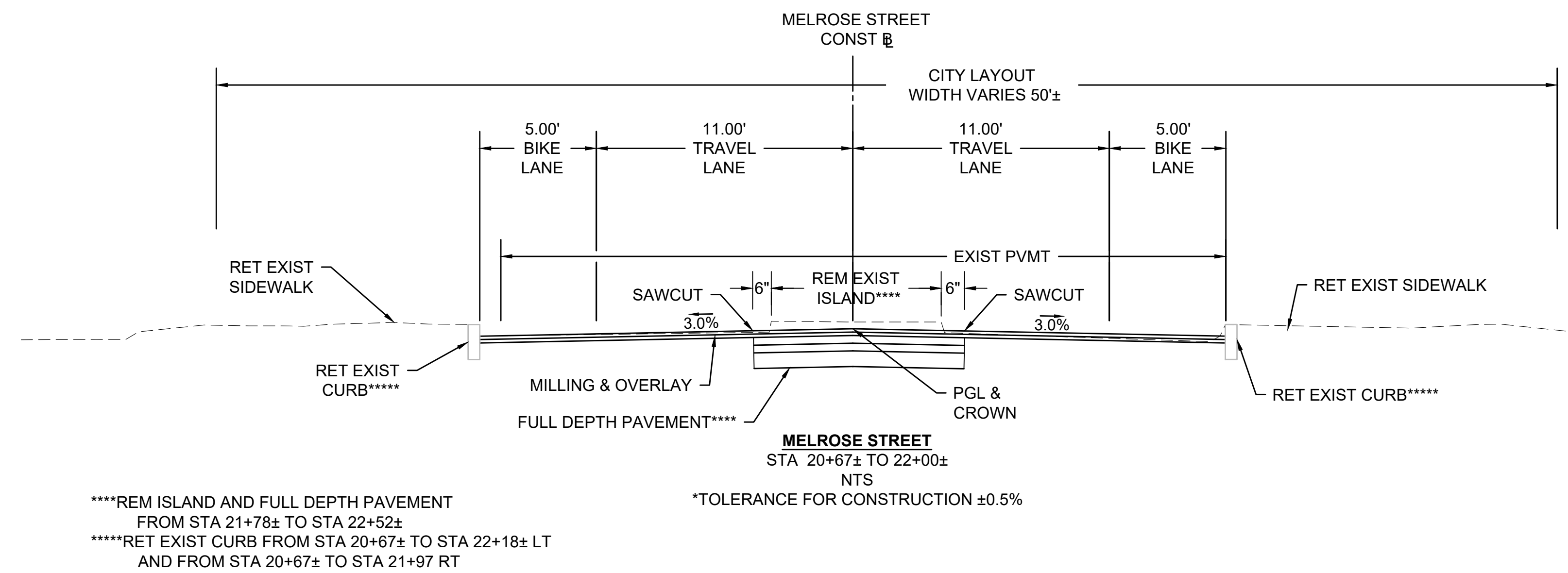
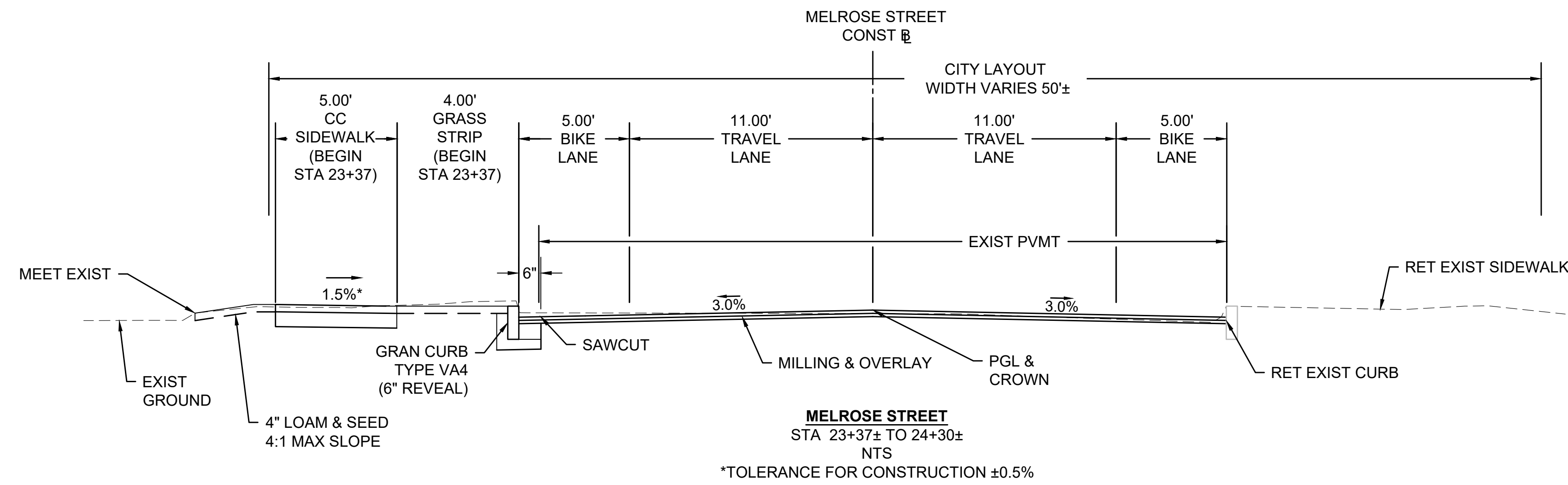
- ALL PAVING SHALL BE IN ACCORDANCE WITH SECTION 450 HOT MIX ASPHALT PAVEMENT AND SECTION M3 ASPHALTIC MATERIALS.
- INTERMEDIATE COURSE SHALL BE PLACED WITHIN 7 DAYS OF THE COMPLETION OF THE PAVEMENT MILLING TO PROTECT THE MILLED SURFACE.
- DRAINAGE INLETS AT ROADWAY LOW POINTS SHALL BE ADJUSTED TO INTERMEDIATE COURSE GRADE IMMEDIATELY FOLLOWING MILLING.
- DRIVEWAY PAVEMENT PAID FOR UNDER ITEM 702.
- HMA FOR PATCHING (ITEM 451.) SHALL BE USED FOR PERMANENT HMAPATCHES. TEMPORARY ASPHALT PATCHING (ITEM 472.) SHALL BE USED FOR TEMPORARY PATCHES.
- PAVEMENT MILLING SHALL ESTABLISH THE PROPOSED GRADING AND A PROPOSED 2.00% CROSS SLOPE, UNLESS OTHERWISE NOTED.

COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
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DIVISION OF PLANNING AND RESOURCE PROTECTION

INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

<small>SURVEY BY: NITSCH</small>	TYPICAL SECTIONS	<small>SHEET NO.</small>
<small>DRAWN BY: KC</small>		6
<small>CHECKED BY: BG</small>	<small>CONT. P20-3361-D1A</small>	<small>SCALE: AS NOTED</small>
<small>APPROVED BY: JM</small>		<small>DATE: 4/11/2023</small>
		<small>6 OF 33</small>

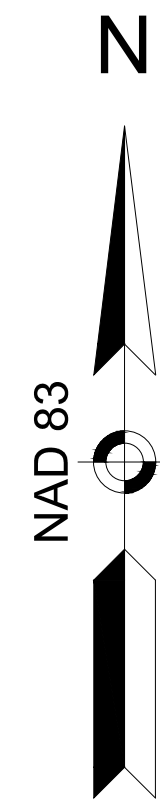
SEE SHEET 6 FOR PAVEMENT NOTES:



**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION**

INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

TYPICAL SECTIONS		SHEET NO. 7
SURVEY BY: NTSCH	DRAWN BY: KC	SCALE: AS NOTED
CHECKED BY: BG	APPROVED BY: JM	DATE: 4/11/2023
CONT. P20-3361-D1A		7 OF 33

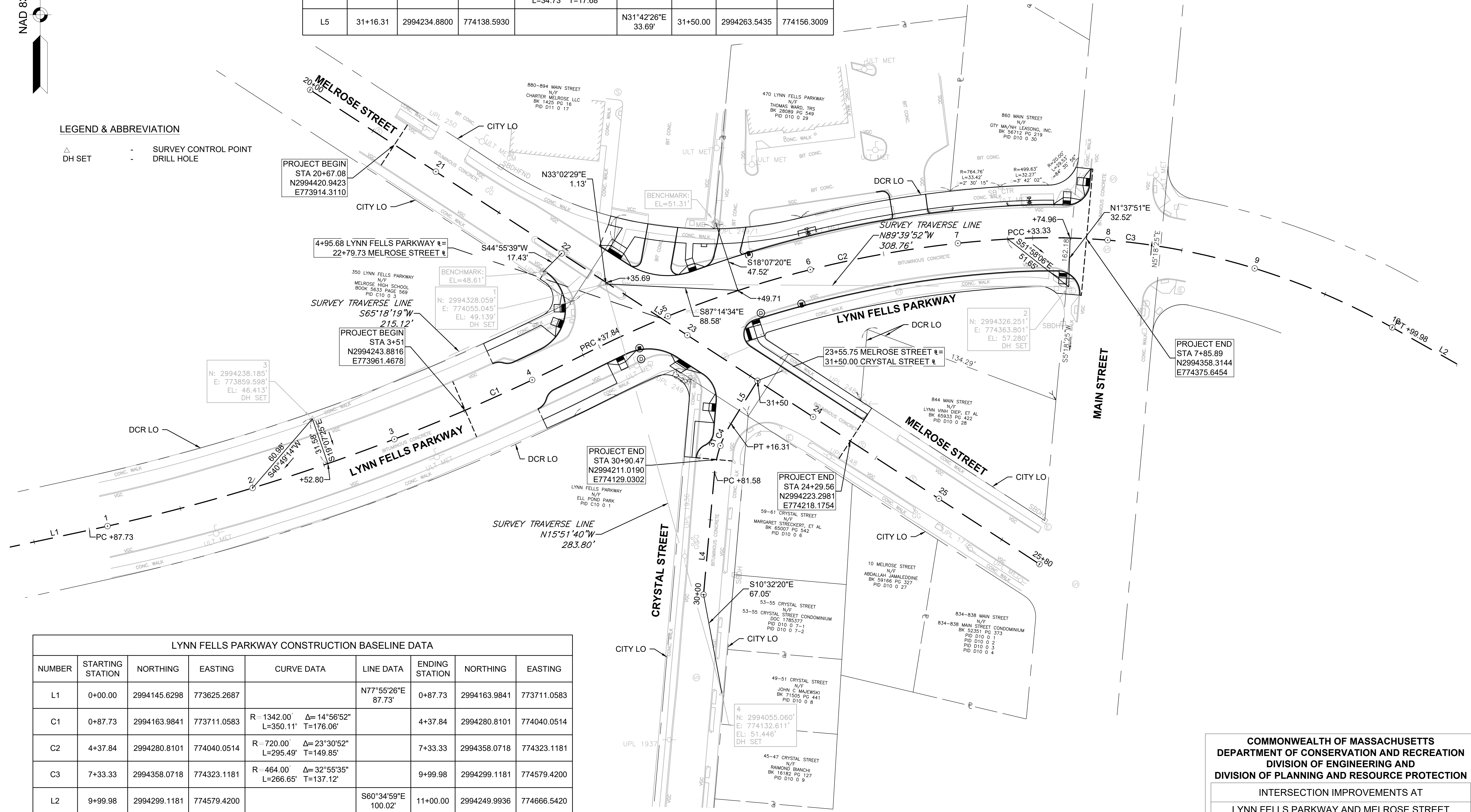


CRYSTAL STREET CONSTRUCTION BASELINE DATA							
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	EASTING
L4	30+00.00	2994120.9815	774120.3474		N5°10'28"E 81.58'	30+81.58	2994202.2258
C4	30+81.58	2994202.2258	774127.7046	R=75.00' Δ=26°31'58" L=34.73' T=17.68'		31+16.31	2994234.8800
L5	31+16.31	2994234.8800	774138.5930		N31°42'26"E 33.69'	31+50.00	2994263.5435

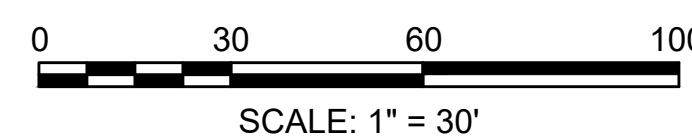
MELROSE STREET CONSTRUCTION BASELINE DATA							
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	EASTING
L3	20+00.00	2994457.5151	773858.0828		S56°57'31"E 580.00'	25+80.00	2994141.2730

LEGEND & ABBREVIATION

- △ SURVEY CONTROL POINT
- DH SET DRILL HOLE
- SURVEY CONTROL POINT
- DRILL HOLE



LYNN FELS PARKWAY CONSTRUCTION BASELINE DATA							
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	EASTING
L1	0+00.00	2994145.6298	773625.2687		N77°55'26"E 87.73'	0+87.73	2994163.9841
C1	0+87.73	2994163.9841	773711.0583	R=1342.00' Δ=14°56'52" L=350.11' T=176.06'		4+37.84	2994280.8101
C2	4+37.84	2994280.8101	774040.0514	R=720.00' Δ=23°30'52" L=295.49' T=149.85'		7+33.33	2994358.0718
C3	7+33.33	2994358.0718	774323.1181	R=464.00' Δ=32°55'35" L=266.65' T=137.12'		9+99.98	2994299.1181
L2	9+99.98	2994299.1181	774579.4200		S60°34'59"E 100.02'	11+00.00	2994249.9936

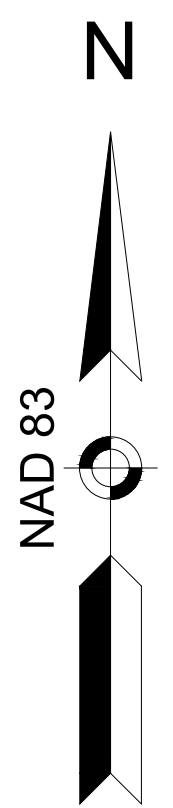
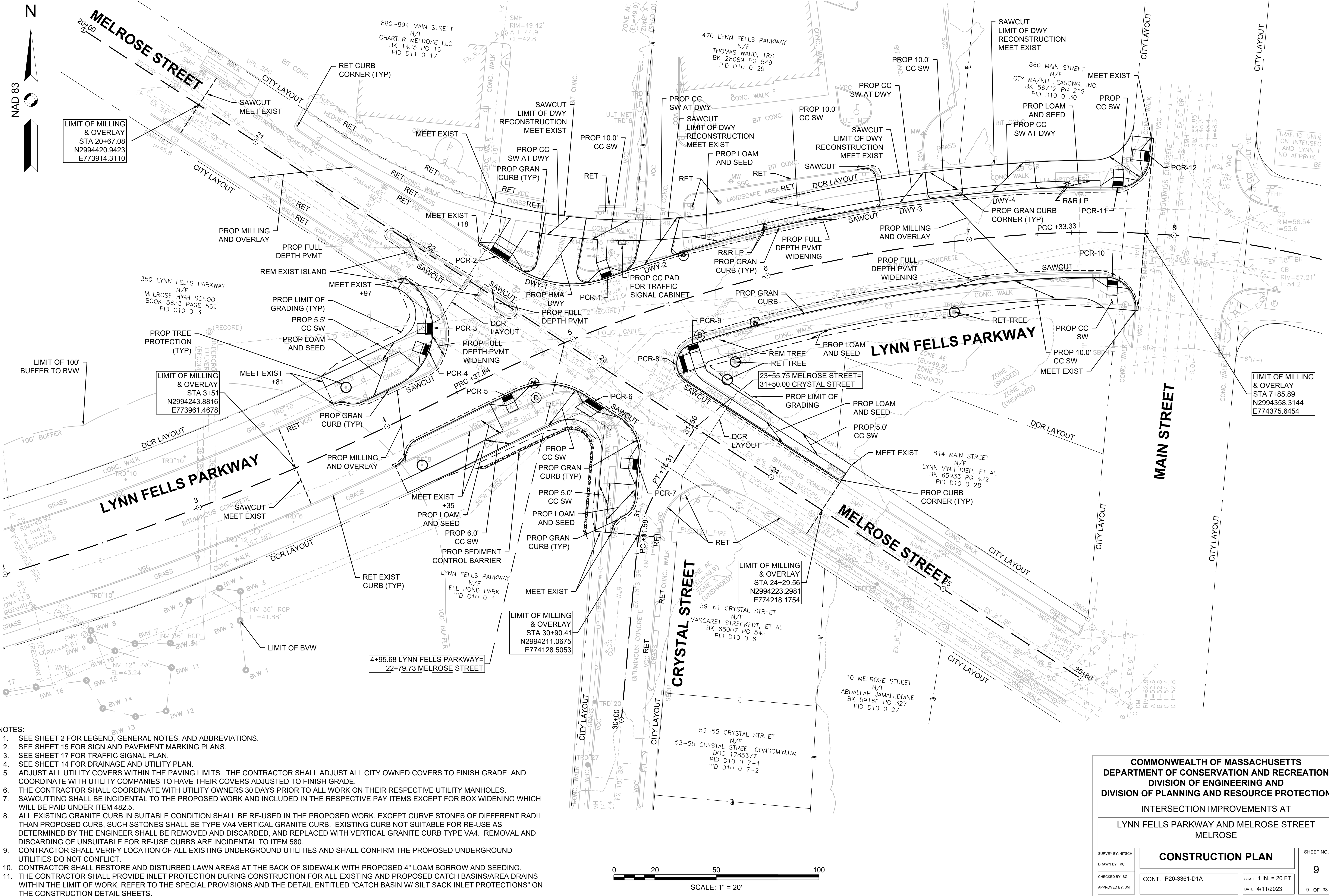


**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION**

**INTERSECTION IMPROVEMENTS AT
LYNN FELS PARKWAY AND MELROSE STREET
MELROSE**

SURVEY BY: NITSCH	SHEET NO. 8
DRAWN BY: KC	CONSTRUCTION BASELINE TIES PLAN
CHECKED BY: BG	
APPROVED BY: JM	
CONT. P20-3361-D1A	SCALE: 1 IN. = 30 FT.
	DATE: 4/11/2023
	8 OF 33

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LIMIT OF MILLING & OVERLAY
 STA 20+67.08
 N2994420.9423
 E773914.3110

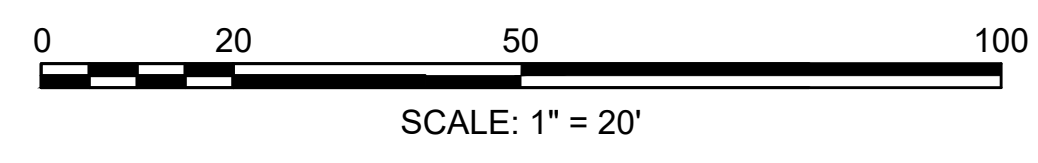
LIMIT OF MILLING & OVERLAY
 STA 3+51
 N2994243.8816
 E773961.4678

LIMIT OF MILLING & OVERLAY
 STA 30+90.41
 N2994211.0675
 E774128.5053

LIMIT OF MILLING & OVERLAY
 STA 24+29.56
 N2994223.2981
 E774218.1754

LIMIT OF MILLING & OVERLAY
 STA 7+85.89
 N2994358.3144
 E774375.6454

- NOTES:
- SEE SHEET 2 FOR LEGEND, GENERAL NOTES, AND ABBREVIATIONS.
 - SEE SHEET 15 FOR SIGN AND PAVEMENT MARKING PLANS.
 - SEE SHEET 17 FOR TRAFFIC SIGNAL PLAN.
 - SEE SHEET 14 FOR DRAINAGE AND UTILITY PLAN.
 - ADJUST ALL UTILITY COVERS WITHIN THE PAVING LIMITS. THE CONTRACTOR SHALL ADJUST ALL CITY OWNED COVERS TO FINISH GRADE, AND COORDINATE WITH UTILITY COMPANIES TO HAVE THEIR COVERS ADJUSTED TO FINISH GRADE.
 - THE CONTRACTOR SHALL COORDINATE WITH UTILITY OWNERS 30 DAYS PRIOR TO ALL WORK ON THEIR RESPECTIVE UTILITY MANHOLES.
 - SAWCUTTING SHALL BE INCIDENTAL TO THE PROPOSED WORK AND INCLUDED IN THE RESPECTIVE PAY ITEMS EXCEPT FOR BOX WIDENING WHICH WILL BE PAID UNDER ITEM 482.5.
 - ALL EXISTING GRANITE CURB IN SUITABLE CONDITION SHALL BE RE-USED IN THE PROPOSED WORK, EXCEPT CURVE STONES OF DIFFERENT RADII THAN PROPOSED CURB, SUCH STONES SHALL BE TYPE VA4 VERTICAL GRANITE CURB. EXISTING CURB NOT SUITABLE FOR RE-USE AS DETERMINED BY THE ENGINEER SHALL BE REMOVED AND DISCARDED, AND REPLACED WITH VERTICAL GRANITE CURB TYPE VA4. REMOVAL AND DISCARDING OF UNSUITABLE FOR RE-USE CURBS ARE INCIDENTAL TO ITEM 580.
 - CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UNDERGROUND UTILITIES AND SHALL CONFIRM THE PROPOSED UNDERGROUND UTILITIES DO NOT CONFLICT.
 - CONTRACTOR SHALL RESTORE AND DISTURBED LAWN AREAS AT THE BACK OF SIDEWALK WITH PROPOSED 4" LOAM BORROW AND SEEDING.
 - THE CONTRACTOR SHALL PROVIDE INLET PROTECTION DURING CONSTRUCTION FOR ALL EXISTING AND PROPOSED CATCH BASINS/AREA DRAINS WITHIN THE LIMIT OF WORK. REFER TO THE SPECIAL PROVISIONS AND THE DETAIL ENTITLED "CATCH BASIN W/ SILT SACK INLET PROTECTIONS" ON THE CONSTRUCTION DETAIL SHEETS.



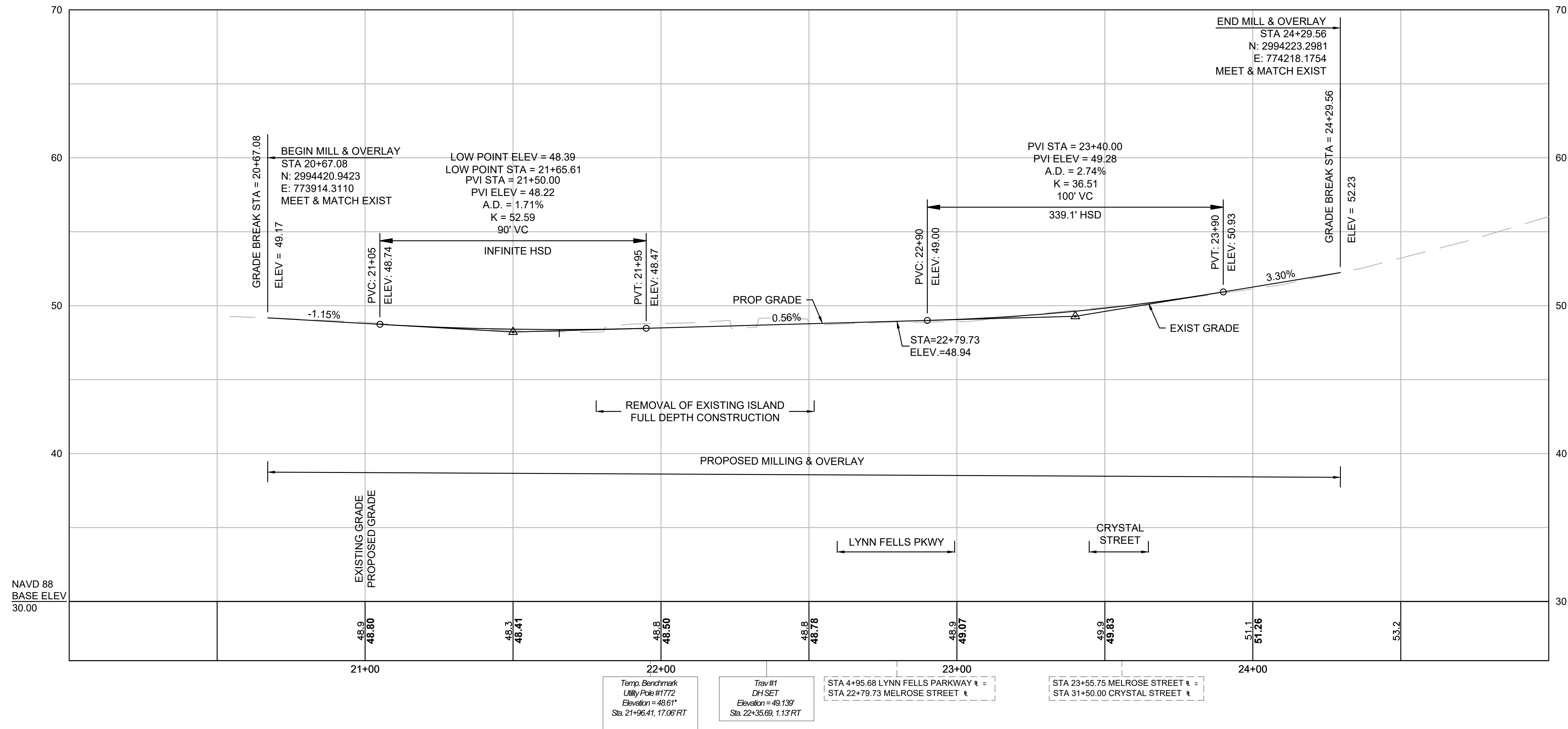
**COMMONWEALTH OF MASSACHUSETTS
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 DIVISION OF ENGINEERING AND
 DIVISION OF PLANNING AND RESOURCE PROTECTION**

INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

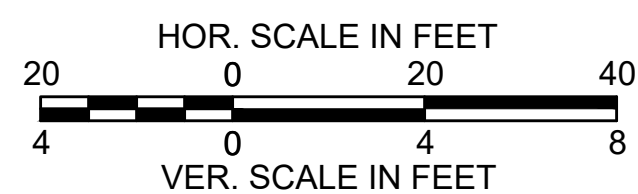
SURVEY BY: NTSCH	CONSTRUCTION PLAN	SHEET NO.
DRAWN BY: KC		9
CHECKED BY: BG	CONT. P20-3361-D1A	SCALE: 1 IN. = 20 FT.
APPROVED BY: JM		DATE: 4/11/2023
		9 OF 33

12589.9_HD(CONSTRUCTION PLAN).DWG Plotted on 3-Aug-2023 2:52 PM

MELROSE STREET



FOR CONSTRUCTION PLANS: SEE SHEET NOS. 9

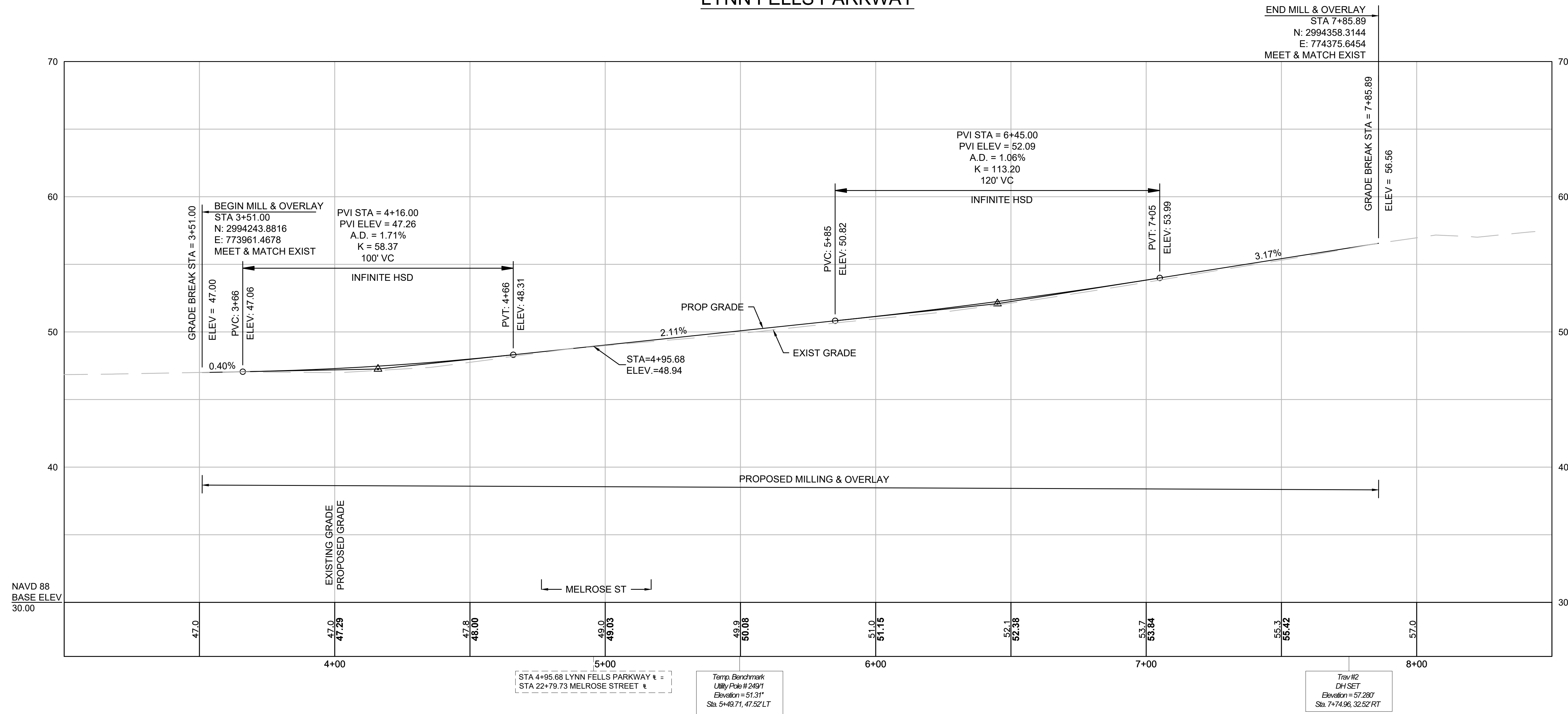


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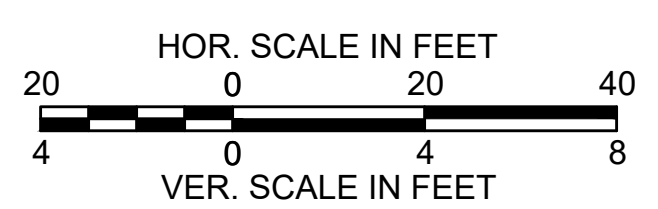
INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

SURVEY BY: NITSCH	PROFILE	SHEET NO.	
DRAWN BY: KC		10	
CHECKED BY: BG		CONT. P20-3361-D1A	SCALE: AS NOTED
APPROVED BY: JM		DATE: 4/11/2023	10 OF 33

LYNN FELLS PARKWAY



FOR CONSTRUCTION PLANS: SEE SHEET NOS. 9



COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
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INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

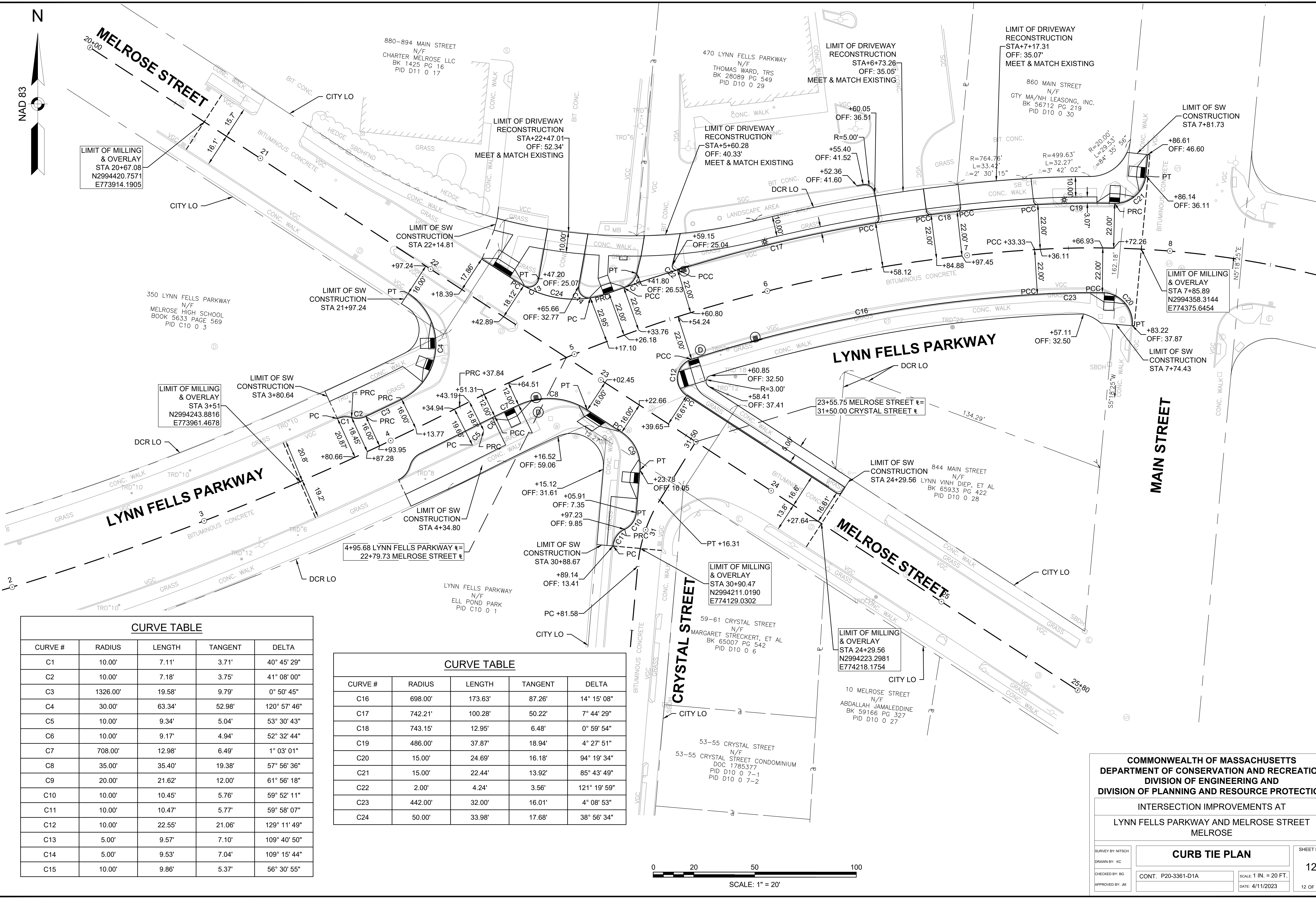
PROFILE	SHEET NO. 11
SURVEY BY: NITSCH	SCALE: AS NOTED DATE: 4/11/2023
DRAWN BY: KC	
CHECKED BY: BG	
APPROVED BY: JM	CONT: P20-3361-D1A 11 OF 33

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12589.9_HD(PROFILE).DWG Plotted on 3-Aug-2023 6:53 AM

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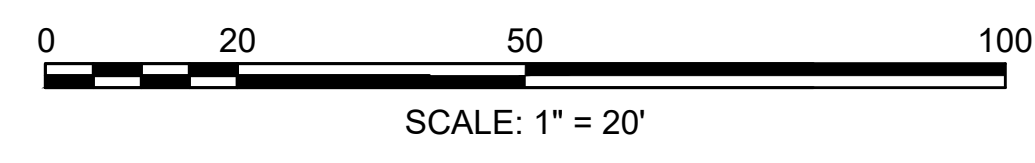
12589.9_HD(CURB TIE PLAN).DWG Plotted on 3-Aug-2023 8:53 AM



CURVE #	RADIUS	LENGTH	TANGENT	DELTA
C1	10.00'	7.11'	3.71'	40° 45' 29"
C2	10.00'	7.18'	3.75'	41° 08' 00"
C3	1326.00'	19.58'	9.79'	0° 50' 45"
C4	30.00'	63.34'	52.98'	120° 57' 46"
C5	10.00'	9.34'	5.04'	53° 30' 43"
C6	10.00'	9.17'	4.94'	52° 32' 44"
C7	708.00'	12.98'	6.49'	1° 03' 01"
C8	35.00'	35.40'	19.38'	57° 56' 36"
C9	20.00'	21.62'	12.00'	61° 56' 18"
C10	10.00'	10.45'	5.76'	59° 52' 11"
C11	10.00'	10.47'	5.77'	59° 58' 07"
C12	10.00'	22.55'	21.06'	129° 11' 49"
C13	5.00'	9.57'	7.10'	109° 40' 50"
C14	5.00'	9.53'	7.04'	109° 15' 44"
C15	10.00'	9.86'	5.37'	56° 30' 55"

CURVE TABLE

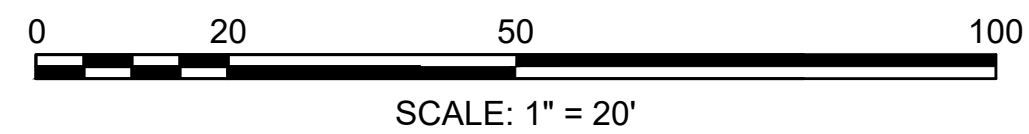
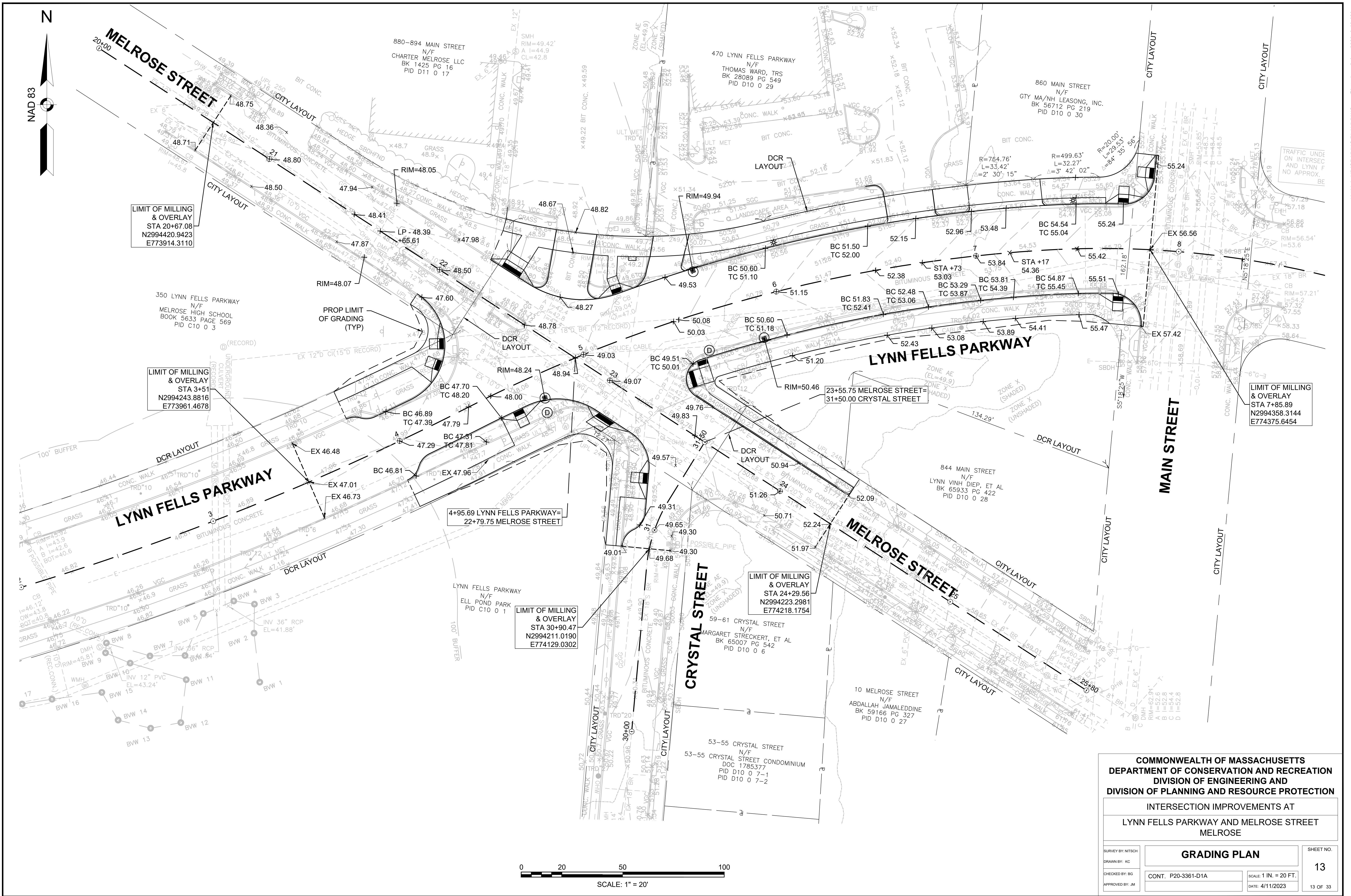
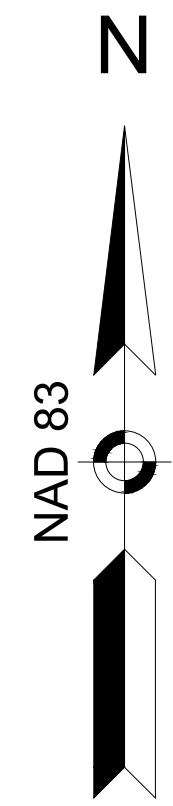
CURVE #	RADIUS	LENGTH	TANGENT	DELTA
C16	698.00'	173.63'	87.26'	14° 15' 08"
C17	742.21'	100.28'	50.22'	7° 44' 29"
C18	743.15'	12.95'	6.48'	0° 59' 54"
C19	486.00'	37.87'	18.94'	4° 27' 51"
C20	15.00'	24.69'	16.18'	94° 19' 34"
C21	15.00'	22.44'	13.92'	85° 43' 49"
C22	2.00'	4.24'	3.56'	121° 19' 59"
C23	442.00'	32.00'	16.01'	4° 08' 53"
C24	50.00'	33.98'	17.68'	38° 56' 34"



**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
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INTERSECTION IMPROVEMENTS AT
**LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE**

CURB TIE PLAN	SHEET NO. 12
SURVEY BY: NITSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	CONT. P20-3361-D1A SCALE: 1 IN. = 20 FT. DATE: 4/11/2023 12 OF 33

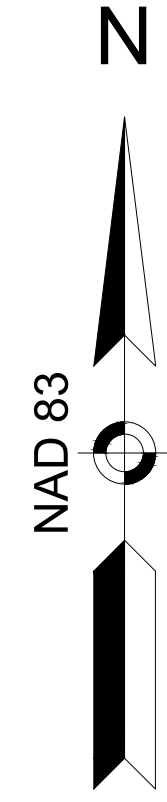


**COMMONWEALTH OF MASSACHUSETTS
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INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

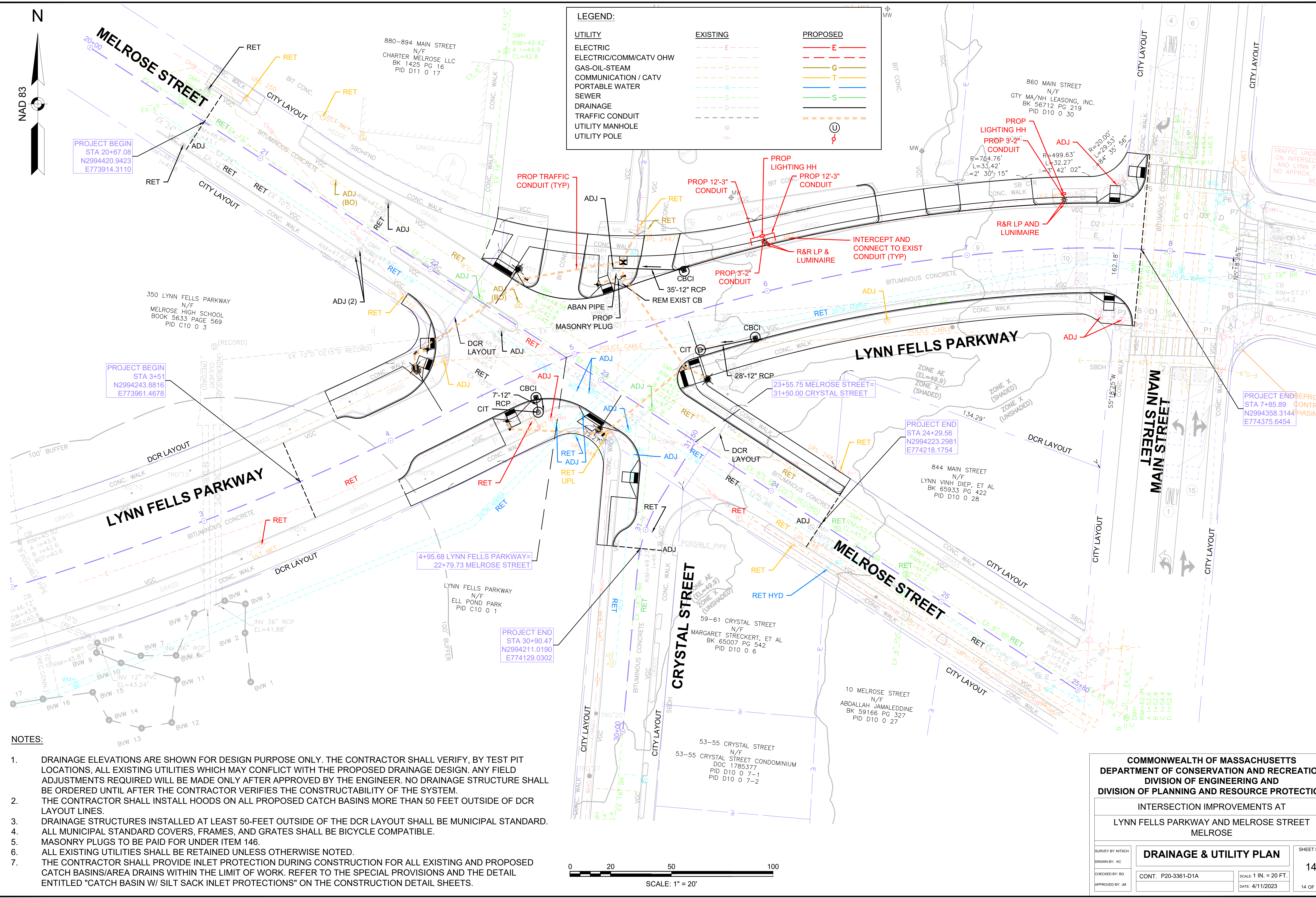
SURVEY BY: NITSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	GRADING PLAN		SHEET NO. 13 13 OF 33
	CONT. P20-3361-D1A	SCALE: 1 IN. = 20 FT. DATE: 4/11/2023	

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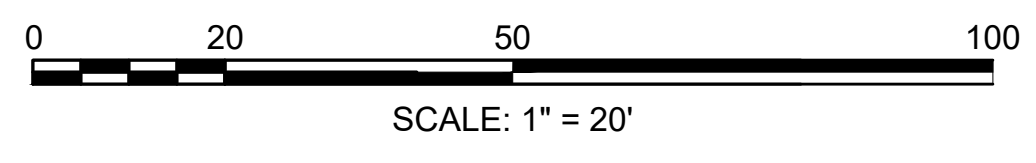


LEGEND:

UTILITY	EXISTING	PROPOSED
ELECTRIC	---	---
ELECTRIC/COMM/CATV OHW	---	---
GAS-OIL-STEAM	---	---
COMMUNICATION / CATV	---	---
PORTABLE WATER	---	---
SEWER	---	---
DRAINAGE	---	---
TRAFFIC CONDUIT	---	---
UTILITY MANHOLE	---	---
UTILITY POLE	---	---



- NOTES:**
- DRAINAGE ELEVATIONS ARE SHOWN FOR DESIGN PURPOSE ONLY. THE CONTRACTOR SHALL VERIFY, BY TEST PIT LOCATIONS, ALL EXISTING UTILITIES WHICH MAY CONFLICT WITH THE PROPOSED DRAINAGE DESIGN. ANY FIELD ADJUSTMENTS REQUIRED WILL BE MADE ONLY AFTER APPROVED BY THE ENGINEER. NO DRAINAGE STRUCTURE SHALL BE ORDERED UNTIL AFTER THE CONTRACTOR VERIFIES THE CONSTRUCTABILITY OF THE SYSTEM.
 - THE CONTRACTOR SHALL INSTALL HOODS ON ALL PROPOSED CATCH BASINS MORE THAN 50 FEET OUTSIDE OF DCR LAYOUT LINES.
 - DRAINAGE STRUCTURES INSTALLED AT LEAST 50- FEET OUTSIDE OF THE DCR LAYOUT SHALL BE MUNICIPAL STANDARD.
 - ALL MUNICIPAL STANDARD COVERS, FRAMES, AND GRATES SHALL BE BICYCLE COMPATIBLE.
 - MASONRY PLUGS TO BE PAID FOR UNDER ITEM 146.
 - ALL EXISTING UTILITIES SHALL BE RETAINED UNLESS OTHERWISE NOTED.
 - THE CONTRACTOR SHALL PROVIDE INLET PROTECTION DURING CONSTRUCTION FOR ALL EXISTING AND PROPOSED CATCH BASIN/AREA DRAINS WITHIN THE LIMIT OF WORK. REFER TO THE SPECIAL PROVISIONS AND THE DETAIL ENTITLED "CATCH BASIN W/ SILT SACK INLET PROTECTIONS" ON THE CONSTRUCTION DETAIL SHEETS.



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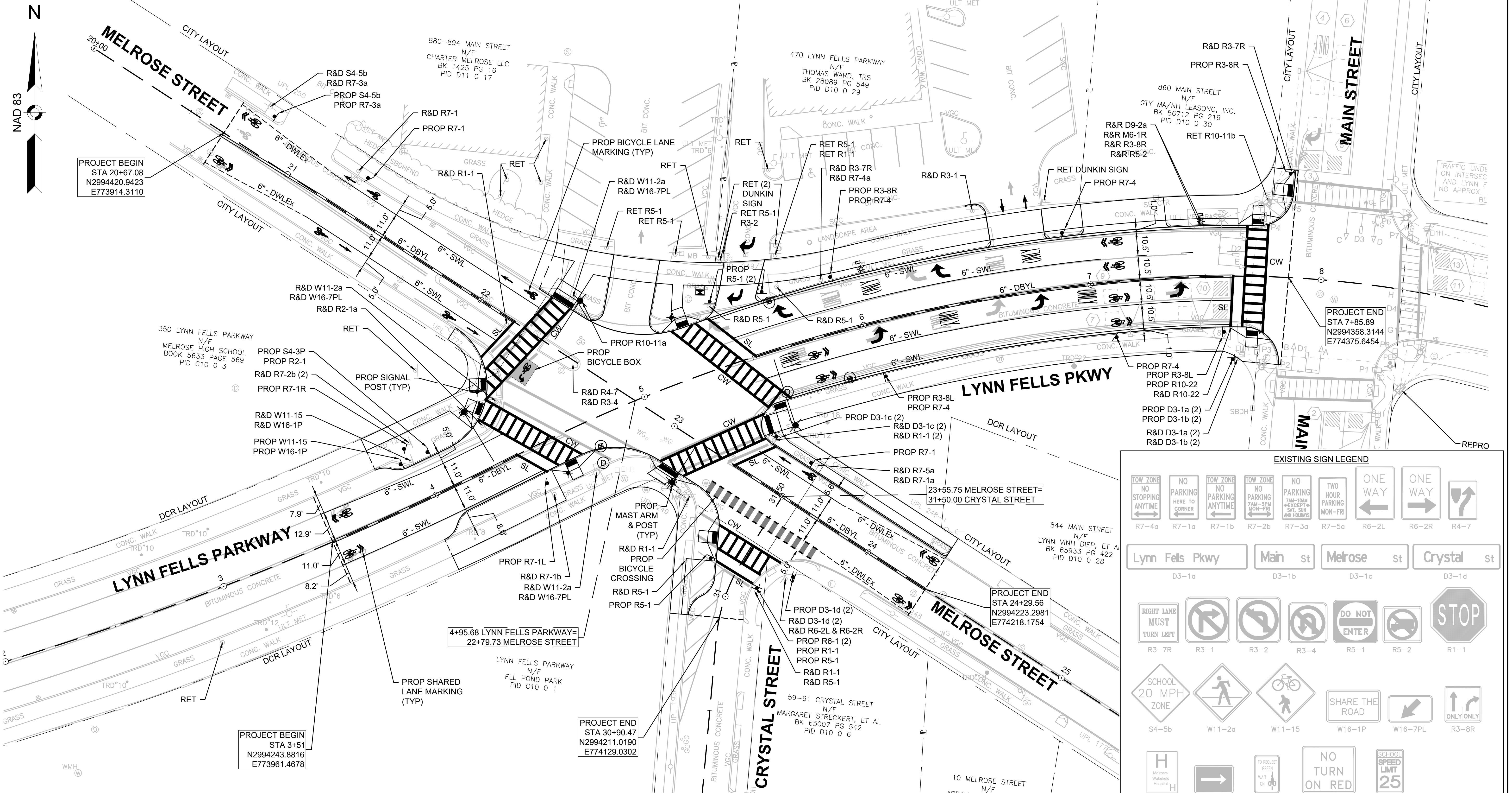
INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

SURVEY BY: NITSCH	DRAINAGE & UTILITY PLAN	SHEET NO. 14
DRAWN BY: KC		
CHECKED BY: BG		
APPROVED BY: JM	CONT. P20-3361-D1A	SCALE: 1 IN. = 20 FT. DATE: 4/11/2023

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PROJECT BEGIN
STA 20+67.08
N2994420.9423
E773914.3110

PROJECT END
STA 7+85.89
N2994358.3144
E774375.6454

PROJECT BEGIN
STA 3+51
N2994243.8816
E773961.4678

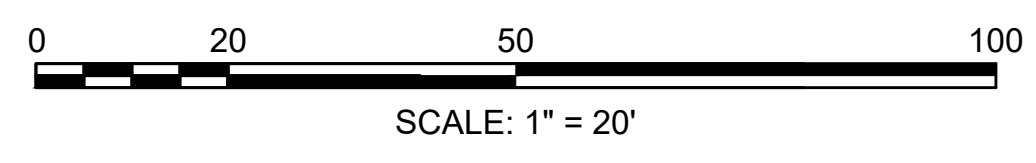
4+95.68 LYNN FELLS PARKWAY=
22+79.73 MELROSE STREET

PROJECT END
STA 24+29.56
N2994223.2981
E774218.1754

PROJECT END
STA 30+90.47
N2994211.0190
E774129.0302

TRAFFIC SIGN AND PAVEMENT MARKINGS NOTES

1. ALL EXISTING SIGNS AND SIGN POSTS WITHIN THE PROJECT LIMITS SHALL REMAIN UNLESS OTHERWISE NOTED ON THE PLANS.
2. ALL PROPOSED PAVEMENT MARKINGS WITHIN THE LIMIT OF WORK SHALL BE THERMOPLASTIC. ALL EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE PROPOSED PAVEMENT MARKINGS SHALL BE REMOVED.
3. ALL PROPOSED SIGN POSTS SHALL BE P-5 TYPE (TELESCOPIC POST) UNLESS NOTED OTHERWISE.
4. PROPOSED PAVEMENT MARKINGS (LEGENDS & ARROWS) SHALL BE INSTALLED IN ACCORDANCE WITH THE LATEST EDITION OF MUTCD & MASSDOT STANDARD DRAWINGS.
5. ALL PROPOSED SIGNS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF MUTCD AND MASSDOT STANDARDS.
6. EXACT LOCATIONS OF PROPOSED SIGNS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
7. ALL SIGN PANELS SHALL BE 90° TO THE CURB AND FACING THE FLOW OF TRAFFIC EXCEPT PARKING REGULATION SIGNS.
8. ALL SIGNS TO BE R&R SHALL BE MOUNTED ON NEW POSTS, UNLESS OTHERWISE NOTED.
9. SIGNS TO BE MOUNTED NEAR THE CURB LINE SHALL BE SET BACK 12" FROM THE EDGE OF THE SIGN PANEL TO THE CURB LINE. NO SIGN SHALL OVERHANG THE CURB LINE.
10. ALL SIGNS SHALL BE MOUNTED TO PROVIDE A 7.0' MINIMUM CLEARANCE BETWEEN THE BOTTOM OF THE SIGN AND FINISH GRADE EXCEPT OBJECT MARKER SIGNS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TREE TRIMMING WHERE NECESSARY TO IMPROVE VISIBILITY OF PROPOSED SIGNAGES.
12. ALL SIGNS AND PAVEMENT MARKINGS BEYOND THE LIMITS OF WORK SHALL BE RETAINED.
13. SEE SHEET 16 FOR PROPOSED TRAFFIC SIGN LEGENDS.
14. SEE SHEET 2 FOR LEGENDS, ABBREVIATIONS, AND GENERAL NOTES.



EXISTING SIGN LEGEND

R7-4a	R7-1a	R7-1b	R7-2b	R7-3a	R7-5a	R6-2L	R6-2R	R4-7

R3-7R	R3-1	R3-2	R3-4	R5-1	R5-2	R1-1

S4-5b	W11-2a	W11-15	W16-1P	W16-7PL	R3-8R

D9-2a	M6-1R	R10-22	R10-11b	R2-1a

**COMMONWEALTH OF MASSACHUSETTS
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**INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE**

SURVEY BY: NITSCH	SIGN & PAVEMENT MARKING PLAN	SHEET NO.
DRAWN BY: KC		15
CHECKED BY: BG	CONT. P20-3361-D1A	SCALE: 1 IN. = 20 FT.
APPROVED BY: JM		DATE: 4/11/2023
		15 OF 33

12589.9_HD(SIGN & PAVEMENT MARKING PLAN).DWG Plotted on 3-Aug-2023 9:16 AM

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PROPOSED TRAFFIC SIGN SUMMARY

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR			POST SIZE AND NUMBER REQUIRED ②	UNIT AREA (S.F.)	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND	BORDER			
MA-D3-1a	VARIES	12"		6D/4D	3" 6D/4D	-	2	RED	WHITE	WHITE	P-5 1 REQ'D	-	EACH
MA-D3-1b	VARIES	12"		6D/4D	3" 6D/4D	-	2	RED	WHITE	WHITE	2 MOUNT W/ MA-D3-1a	-	EACH
MA-D3-1c	VARIES	12"		6D/4D	3" 6D/4D	-	2	RED	WHITE	WHITE	P-5 1 REQ'D	-	EACH
MA-D3-1d	VARIES	12"		6D/4D	3" 6D/4D	-	2	RED	WHITE	WHITE	P-5 1 REQ'D	-	EACH
R1-1	30"	30"		①	①	①	1	RED	WHITE	WHITE	P-5 1 REQ'D	6.25	6.25
R2-1 (25)	24"	30"					1	WHITE	BLACK	BLACK	1 MOUNT W/ S4-3P	5.00	5.00
R3-8L	30"	30"					2	WHITE	BLACK	BLACK	1 MOUNT W/ R7-4 1 MOUNT W/ R10-22	6.25	12.50
R3-8R	30"	30"					2	WHITE	BLACK	BLACK	P-5 1 REQ'D 1 MOUNT W/ R7-4	6.25	12.50
R5-1	30"	30"					4	RED	WHITE	WHITE	P-5 3 REQ'D 1 MOUNT W/ R1-1	6.25	25.00
R6-1	36"	12"					2	WHITE	BLACK	BLACK	2 MOUNT W/ R1-1	3.00	6.00
R7-1	12"	18"					2	WHITE	RED	RED	P-5 2 REQ'D	1.50	3.00
R7-1L	12"	18"					1	WHITE	RED	RED	P-5 1 REQ'D	1.50	1.50
R7-1R	12"	18"					1	WHITE	RED	RED	P-5 1 REQ'D	1.50	1.50
R7-3a	12"	18"					1	WHITE	RED	RED	1 MOUNT W/ S4-5b	1.50	1.50
R7-4	12"	18"					4	WHITE	GREEN /BLUE	RED	P-5 4 REQ'D	1.50	6.00
R10-11a	24"	30"					0	WHITE	BLACK	BLACK	4 MOUNT W/ MAST ARM	5.00	20.00
R10-22	12"	18"					1	WHITE	BLACK	BLACK	P-5 1 REQ'D	1.50	1.50
S4-3P	24"	8"					1	FLUOR. YELLOW GREEN	BLACK	BLACK	P-5 1 REQ'D	1.33	1.33
S4-5b	30"	30"					1	FLUOR. YELLOW GREEN	BLACK	BLACK	P-5 1 REQ'D	6.25	6.25

IDENTIFICATION NUMBER	SIZE OF SIGN		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR			POST SIZE AND NUMBER REQUIRED ②	UNIT AREA (S.F.)	AREA IN SQUARE FEET
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW RTE. MKR.		BACK-GROUND	LEGEND	BORDER			
W11-15	30"	30"		①	①	①	1	YELLOW	BLACK	BLACK	P-5 1 REQ'D	6.25	6.25
W16-1P	18"	24"		↓	↓	↓	1	YELLOW	BLACK	BLACK	1 MOUNT W/ W11-15	3.00	3.00

NOTES

- ① SEE MUTCD 2009 EDITION, THE 2012 SUPPLEMENT TO THE 2004 EDITION OF THE STANDARD HIGHWAY SIGNS AND SECTION M9.30.0 TYPE III OF THE MASSDOT STANDARD SPECIFICATION FOR TEXT DIMENSIONS AND COLOR.
- ② SEE STANDARD DRAWINGS FOR SIGNS AND SUPPORTS, 1990.

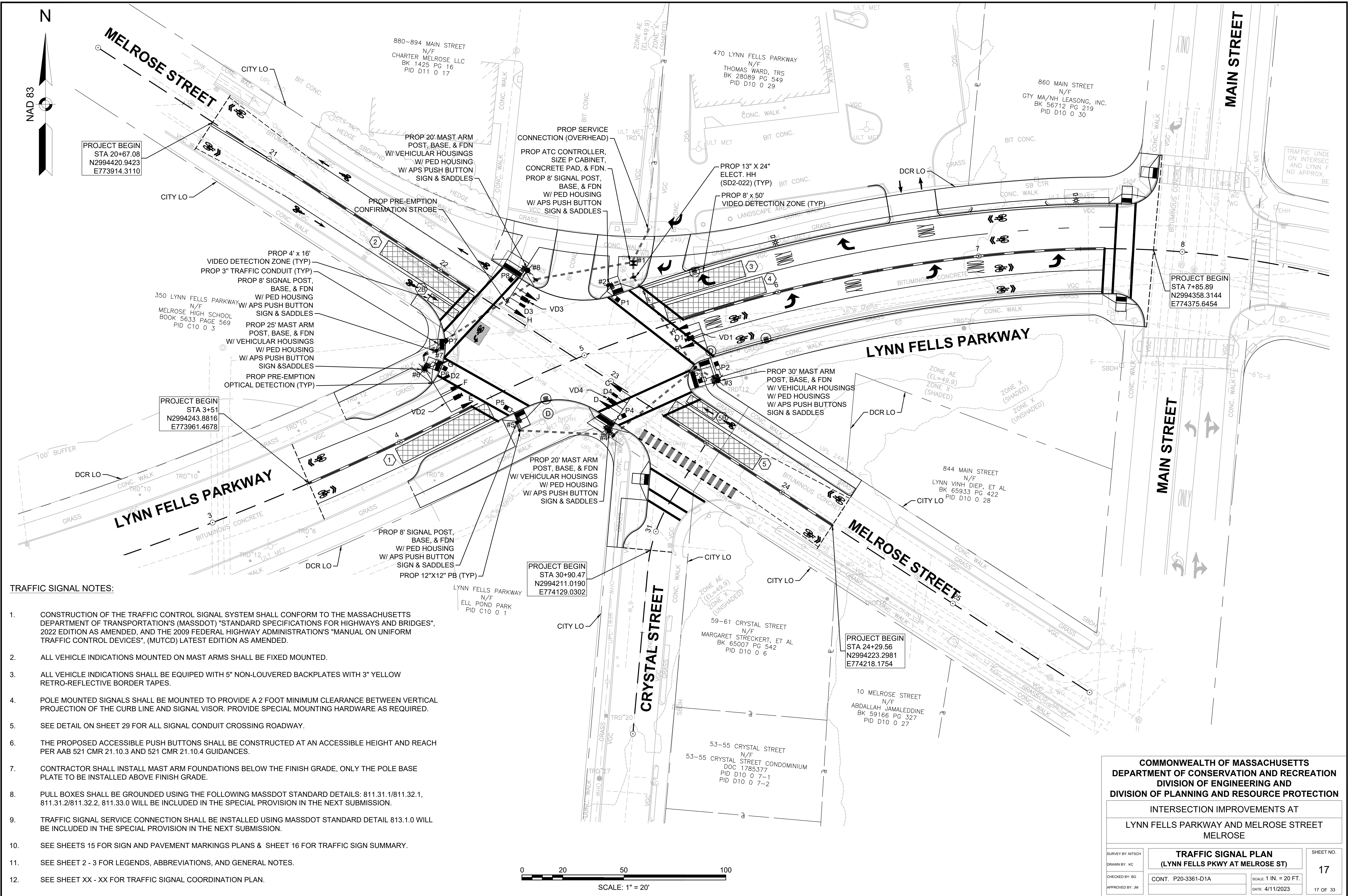
COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION

INTERSECTION IMPROVEMENTS AT
 LYNN FELS PARKWAY AND MELROSE STREET
 MELROSE

SURVEY BY: NITSCH	TRAFFIC SIGN SUMMARY	SHEET NO.
DRAWN BY: KC		16
CHECKED BY: BG	CONT. P20-3361-D1A	SCALE: NTS
APPROVED BY: JM		DATE: 4/11/2023
		16 OF 33

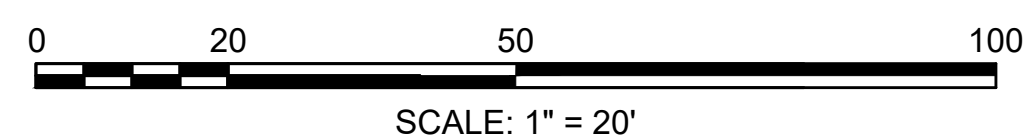
Nitsch - P: \\10000-14999\12589.9\LynnFellsMelrose\Transportation\CAD\12589.9\CAD\Project Drawing Data\DWG\12589.9_HD(SIGNAL PLAN).dwg Aug 3, 2023 10:52 AM

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TRAFFIC SIGNAL NOTES:

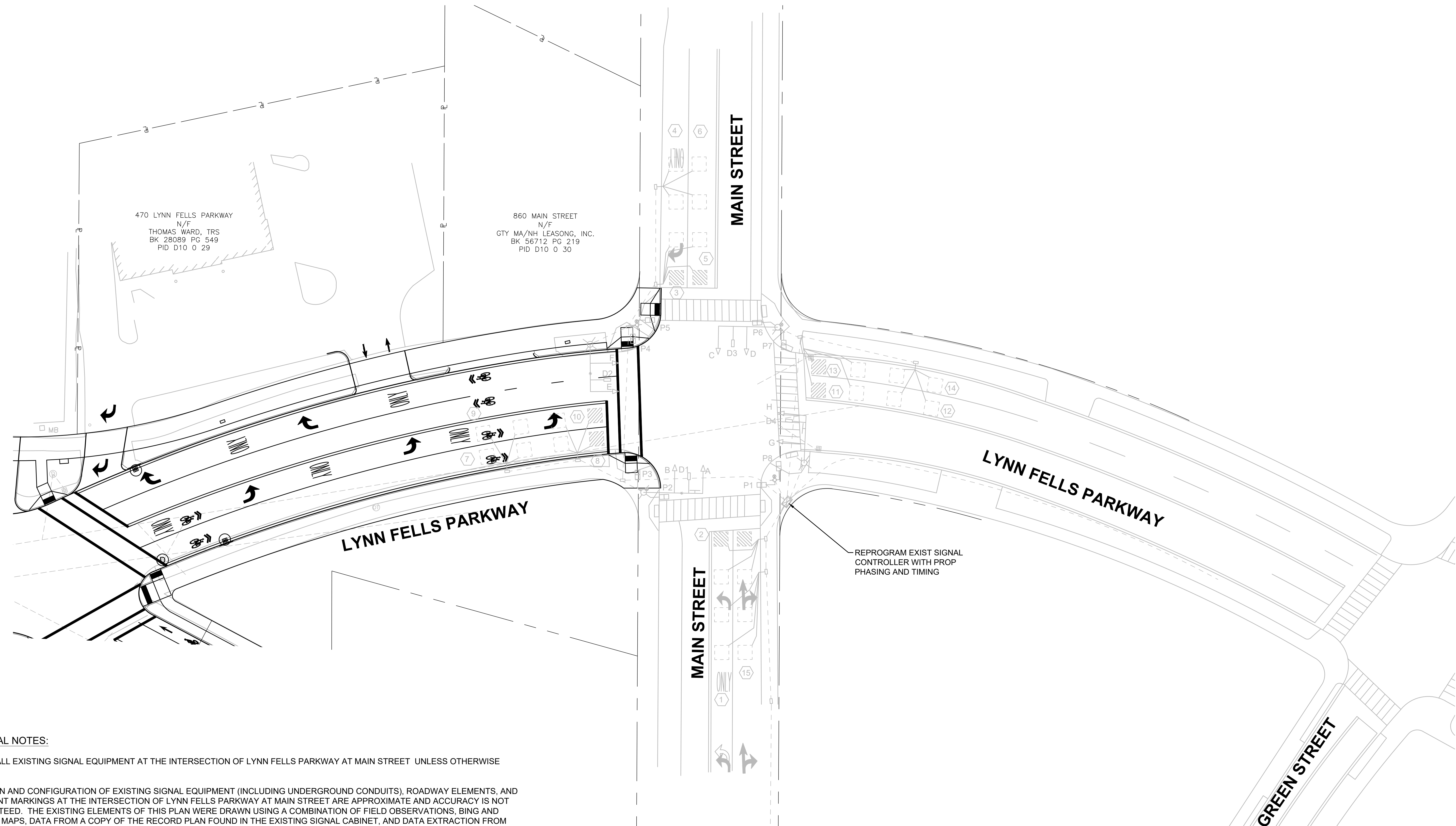
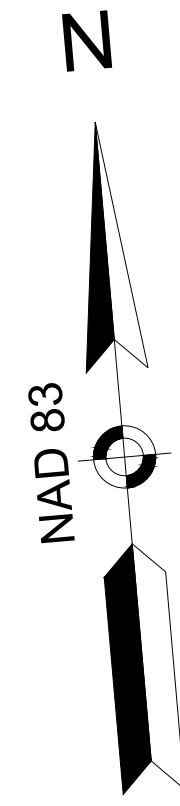
1. CONSTRUCTION OF THE TRAFFIC CONTROL SIGNAL SYSTEM SHALL CONFORM TO THE MASSACHUSETTS DEPARTMENT OF TRANSPORTATION'S (MASSDOT) "STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES", 2022 EDITION AS AMENDED, AND THE 2009 FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", (MUTCD) LATEST EDITION AS AMENDED.
2. ALL VEHICLE INDICATIONS MOUNTED ON MAST ARMS SHALL BE FIXED MOUNTED.
3. ALL VEHICLE INDICATIONS SHALL BE EQUIPPED WITH 5" NON-LOUVERED BACKPLATES WITH 3" YELLOW RETRO-REFLECTIVE BORDER TAPES.
4. POLE MOUNTED SIGNALS SHALL BE MOUNTED TO PROVIDE A 2 FOOT MINIMUM CLEARANCE BETWEEN VERTICAL PROJECTION OF THE CURB LINE AND SIGNAL VISOR. PROVIDE SPECIAL MOUNTING HARDWARE AS REQUIRED.
5. SEE DETAIL ON SHEET 29 FOR ALL SIGNAL CONDUIT CROSSING ROADWAY.
6. THE PROPOSED ACCESSIBLE PUSH BUTTONS SHALL BE CONSTRUCTED AT AN ACCESSIBLE HEIGHT AND REACH PER AAB 521 CMR 21.10.3 AND 521 CMR 21.10.4 GUIDANCES.
7. CONTRACTOR SHALL INSTALL MAST ARM FOUNDATIONS BELOW THE FINISH GRADE, ONLY THE POLE BASE PLATE TO BE INSTALLED ABOVE FINISH GRADE.
8. PULL BOXES SHALL BE GROUNDED USING THE FOLLOWING MASSDOT STANDARD DETAILS: 811.31.1/811.32.1, 811.31.2/811.32.2, 811.33.0 WILL BE INCLUDED IN THE SPECIAL PROVISION IN THE NEXT SUBMISSION.
9. TRAFFIC SIGNAL SERVICE CONNECTION SHALL BE INSTALLED USING MASSDOT STANDARD DETAIL 813.1.0 WILL BE INCLUDED IN THE SPECIAL PROVISION IN THE NEXT SUBMISSION.
10. SEE SHEETS 15 FOR SIGN AND PAVEMENT MARKINGS PLANS & SHEET 16 FOR TRAFFIC SIGN SUMMARY.
11. SEE SHEET 2 - 3 FOR LEGENDS, ABBREVIATIONS, AND GENERAL NOTES.
12. SEE SHEET XX - XX FOR TRAFFIC SIGNAL COORDINATION PLAN.



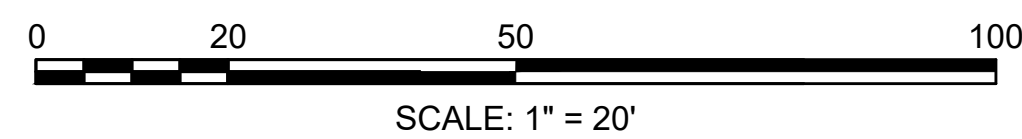
**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION**

INTERSECTION IMPROVEMENTS AT
**LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE**

SURVEY BY: NITSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	TRAFFIC SIGNAL PLAN (LYNN FELLS PKWY AT MELROSE ST)	SHEET NO. 17 17 OF 33
CONT. P20-3361-D1A		SCALE: 1 IN. = 20 FT. DATE: 4/11/2023

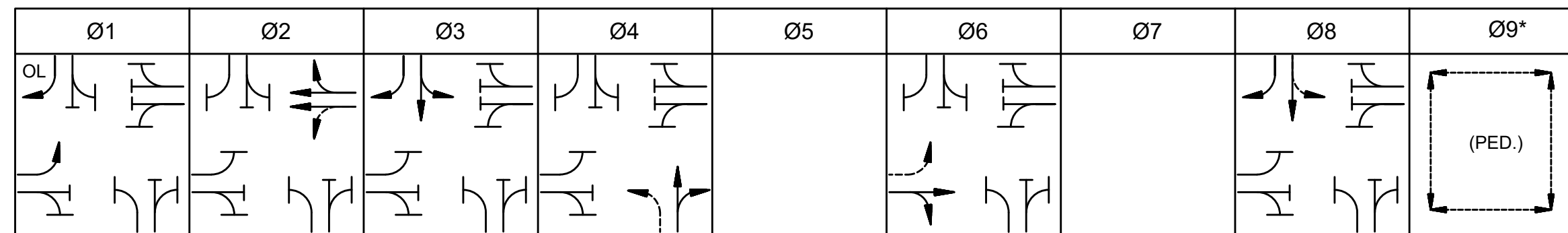
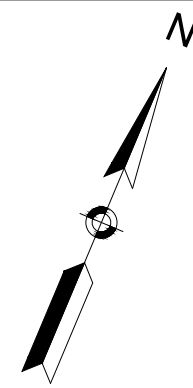


1. RETAIN ALL EXISTING SIGNAL EQUIPMENT AT THE INTERSECTION OF LYNN FELLS PARKWAY AT MAIN STREET UNLESS OTHERWISE NOTED.
2. LOCATION AND CONFIGURATION OF EXISTING SIGNAL EQUIPMENT (INCLUDING UNDERGROUND CONDUITS), ROADWAY ELEMENTS, AND PAVEMENT MARKINGS AT THE INTERSECTION OF LYNN FELLS PARKWAY AT MAIN STREET ARE APPROXIMATE AND ACCURACY IS NOT GUARANTEED. THE EXISTING ELEMENTS OF THIS PLAN WERE DRAWN USING A COMBINATION OF FIELD OBSERVATIONS, BING AND GOOGLE MAPS, DATA FROM A COPY OF THE RECORD PLAN FOUND IN THE EXISTING SIGNAL CABINET, AND DATA EXTRACTION FROM THE EXISTING SIGNAL CONTROLLER.
3. SIGNAL PHASE NUMBERING HAS BEEN CHANGED RELATIVE TO EXISTING.
4. LOOP DETECTORS ON THE SOUTH LEG OF MAIN STREET WERE MODIFIED IN THE TIME SINCE THE 2005 RECORD PLAN AND THE DATE OF THIS PLAN. DETECTOR GROUP NUMBERS ON THIS PLAN MATCH THOSE ON THE RECORD PLAN, WITH THE GROUP ADDED ON THE SOUTH LEG OF MAIN STREET GIVEN THE NEXT AVAILABLE NUMBER, AND THESE NUMBERS MAY NOT MATCH THE EXISTING CONFIGURATION.
5. CONSTRUCTION OF THE TRAFFIC CONTROL SIGNAL SYSTEM SHALL CONFORM TO THE MASSACHUSETTS DEPARTMENT OF TRANSPORTATION'S (MASSDOT) "STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES", 2022 EDITION AS AMENDED, AND THE 2009 FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", (MUTCD) LATEST EDITION AS AMENDED.
6. SEE SHEET 2 - 3 FOR LEGENDS, ABBREVIATIONS, AND GENERAL NOTES.
7. SEE SHEET XX - XX FOR TRAFFIC SIGNAL COORDINATION PLAN.



COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION DIVISION OF ENGINEERING AND DIVISION OF PLANNING AND RESOURCE PROTECTION		
INTERSECTION IMPROVEMENTS AT LYNN FELLS PARKWAY AND MELROSE STREET MELROSE		
SURVEY BY: NTSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	TRAFFIC SIGNAL PLAN (LYNN FELLS PKWY AT MAIN ST) CONT: P20-3361-D1A DATE: 4/11/2023	SHEET NO. 19 SCALE: 1 IN. = 20 FT. DATE: 4/11/2023 19 OF 33

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SEQUENCE AND TIMING FOR FULL ACTUATED CONTROL (COORDINATED)																															
STREET	DIRECTION	HOUSINGS	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	FLASH OPER.	
MAIN STREET	SB	A	R	R	R	R	R	R	GL/G	YLY	R	R	R	R				R	R	R				G	Y	R	R	R	R	FY	
MAIN STREET	SB	B	R/R	R/YR	R	R	R	R	G/GR	Y/YR	R	R	R	R				R	R	R				G	Y	R	R	R	R	FY	
MAIN STREET	NB	C,D	R	R	R	R	R	R	R	R	R	R	G	Y	R			R	R	R				R	R	R	R	R	R	FY	
LYNN FELS PARKWAY	WB	E,F	R	R	R	G	Y	R	R	R	R	R	R	R				R	R	R				R	R	R	R	R	R	FR	
LYNN FELS PARKWAY	EB	G	R	R	R	R	R	R	R	R	R	R	R	R				G	Y	R				R	R	R	R	R	R	FR	
LYNN FELS PARKWAY	EB	H	GL	YL	R	R	R	R	R	R	R	R	R	R				G	Y	R				R	R	R	R	R	R	FR	
PEDESTRIAN CROSSING	EB-WB	P1,P2	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW				DW	DW	DW				DW	DW	DW	W	FDW	DW	OFF	
PEDESTRIAN CROSSING	NB-SB	P3,P4	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW				DW	DW	DW				DW	DW	DW	W	FDW	DW	OFF	
PEDESTRIAN CROSSING	EB-WB	P5,P6	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW				DW	DW	DW				DW	DW	DW	W	FDW	DW	OFF	
PEDESTRIAN CROSSING	NB-SB	P7,P8	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW				DW	DW	DW				DW	DW	DW	W	FDW	DW	OFF	

TIMING IN SECONDS																															
MINIMUM GREEN (INITIAL)				5																											
PASSAGE TIME (VEHICLE)				1			2																								
MAXIMUM 1				5			17																								
MAXIMUM 2				5			19																								
YELLOW CLEARANCE					3			3						3																	
RED CLEARANCE						2			2						2																
WALK (W)																															
PEDESTRIAN CLEARANCE																															10
RECALL					OFF		SOFT		OFF		OFF		OFF					SOFT		OFF		OFF									
MEMORY					NON-LOCKING		NON-LOCKING		NON-LOCKING		NON-LOCKING		NON-LOCKING					NON-LOCKING		NON-LOCKING		LOCKING									

COORDINATION DATA				COORDINATION PHASE TIMING											
TIMING PLAN	CYCLE LENGTH (SEC.)	REF/OFFSET (SEC.)	SEC.	SEC.	SEC.	SEC.	SEC.	SEC.	SEC.	SEC.	SEC.	SEC.	SEC.		
1 (6 AM - 2 PM M-F)	110	0	10	21	30	29			21			29	20		
2 (2 PM - 9 PM M-F)	100	0	10	24	24	22			24			22	20		
3 (9 AM - 9 PM SA-SU)	95	0	10	20	18	27			20			27	20		

FREE OPERATION ALL OTHER TIMES

MAXIMUM 1 GREEN FOR FREE OPERATION.
MAXIMUM 2 GREEN IN EFFECT DURING COORDINATION.
COORDINATED PHASE: Ø 2 & Ø 6 START OF GREEN.
SEE SHEETS XX - XX FOR SIGNAL COORDINATION PLAN.
* PEDESTRIAN PHASE PUSH BUTTON ACTUATED ONLY.

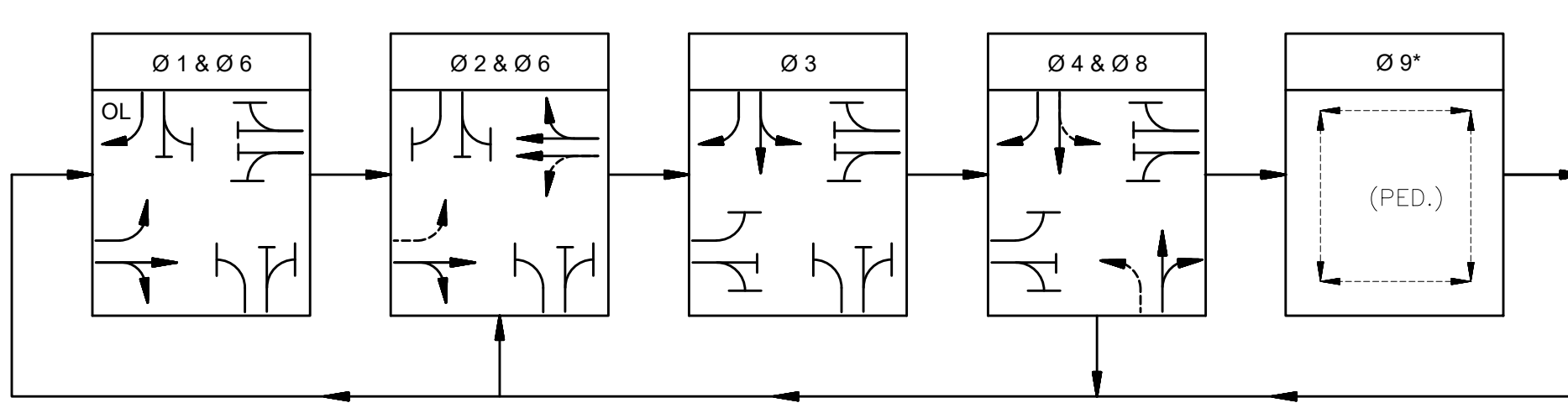
EMERGENCY VEHICLE PREEMPTION SCHEDULE

APPROACH	PREEMPTION RECEIVER	PREEMPTION PHASE	NEXT PHASE CALLED
SOUTHBOUND	D1	3	4 & 8
WESTBOUND	D2	2	2 & 6
NORTHBOUND	D3	4	4 & 8
EASTBOUND	D4	1 & 6	2 & 6

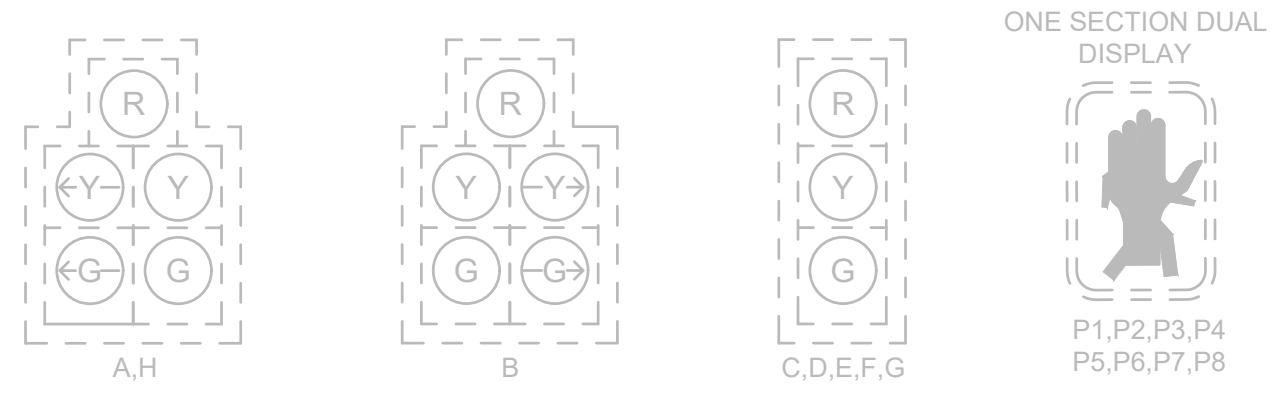
EMERGENCY VEHICLE PREEMPTION OPERATION:

- EMERGENCY VEHICLE PREEMPTION SHALL BE ACTUATED BY AN OPTICAL SIGNAL FROM AN OPTICAL EMITTER MOUNTED ON AN EMERGENCY VEHICLE AND RECEIVED BY AN OPTICAL DETECTOR LOCATED AT INTERSECTION. A SEPARATE RECEIVING DETECTOR IS REQUIRED FOR EACH DETECTED APPROACH.
- PREEMPTION SIGNALS FROM MULTIPLE APPROACHES SHALL BE SERVICED ON A FIRST DETECTED FIRST SERVED BASIS.
- IN RESPONSE TO A PREEMPTION SIGNAL RECEIVED AT AN INTERSECTION BY AN OPTICAL DETECTOR, THE CONTROLLER SHALL TIME THE CLEARANCE INTERVALS OF THE ACTIVE PHASE (IF DIFFERENT THAT TO BE SERVICED) AND ADVANCE TO AND/OR HOLD IN EMERGENCY VEHICLE PREEMPTION PHASE UNTIL PREEMPTION SIGNAL CEASES. THE CONTROLLER SHALL THEN TIME CLEARANCES AND SIMILARLY SERVICE OTHER EMERGENCY VEHICLE PREEMPTION SEQUENCES IN THE ORDER RECEIVED (IF RECEIVED) OTHERWISE, RESUME NORMAL PREFERENTIAL PHASE SEQUENCE.
- PREEMPTION MINIMUM GREENS SHALL BE TEN SECONDS.
- NORMAL CLEARANCES SHALL BE PROVIDED ON PHASES THAT ARE TERMINATED BY PREEMPTION DEMAND.
- ACTUAL TIMING FOR PREEMPTION SHALL BE DETERMINED IN THE FIELD IN COORDINATION WITH THE FIRE DEPARTMENT AND SHALL BE APPROVED BY DCR PRIOR TO OPERATION.

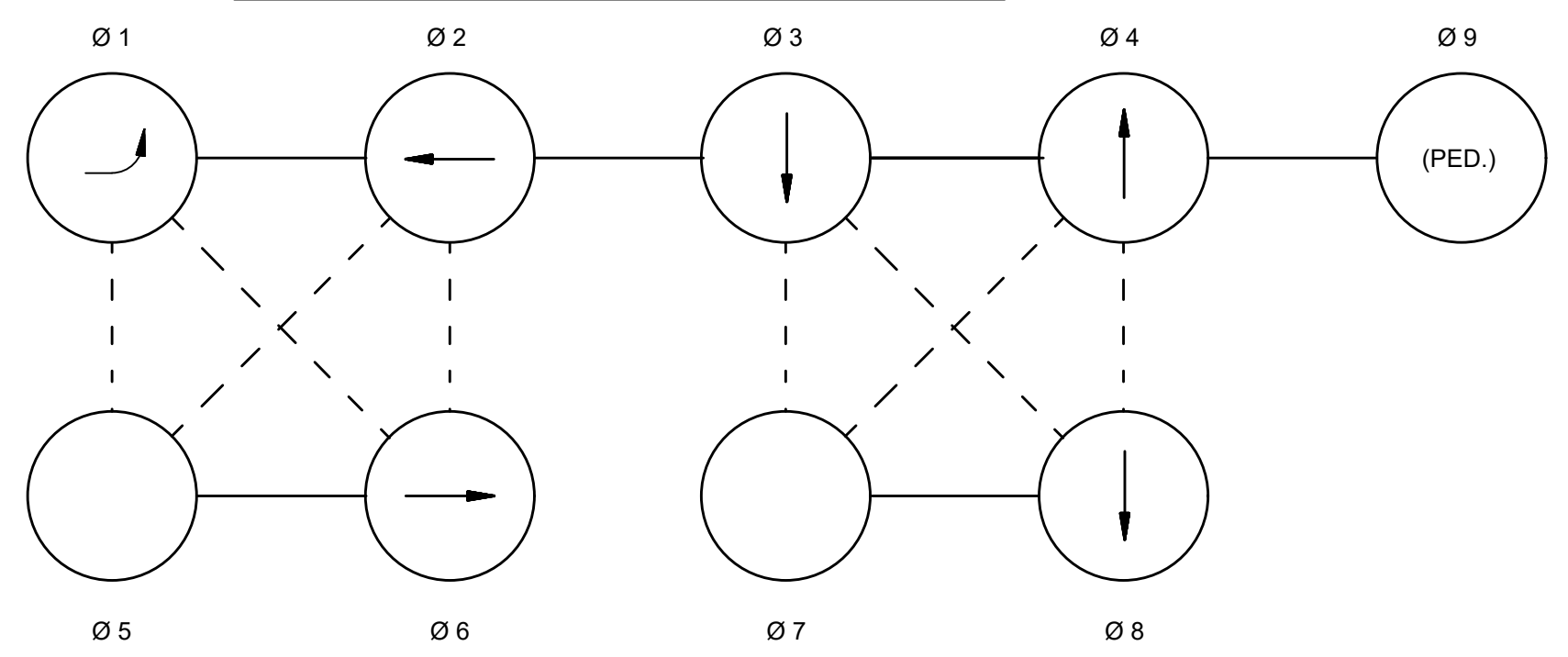
PREFERENTIAL PHASING SEQUENCE



EXISTING SIGNAL IDENTIFICATION



NEMA DUAL RING PHASING NOTES:



- PHASES ASSOCIATED BY A SOLID LINE SHALL NOT OPERATE CONCURRENTLY.
- PHASES ASSOCIATED BY A DASHED LINE MAY OPERATE CONCURRENTLY.
- THROUGH MOVEMENTS MAY INCLUDE RIGHT TURNS.
- IF THE ASSIGNED RIGHT OF WAY FOR ANY TRAFFIC MOVEMENT IS TO REMAIN IN EFFECT DURING THE NEXT CALLED PHASE, THE SIGNAL INDICATIONS FOR THAT TRAFFIC MOVEMENT SHALL NOT CHANGE DURING THE CHANGE INTERVAL(S) UNLESS OTHERWISE NOTED.

MAJOR ITEMS REQUIRED	
PAY ITEM	QUANTITY
1	REPROGRAM EXIST SIGNAL CONTROLLER WITH PROP PHASING AND TIMING
1	GPS TIME UNIT
1	HIGH GAIN DIRECTIONAL CELLULAR ANTENNA
Plus all necessary duct, cable, labor, miscellaneous material and equipment to complete the installation.	

LOOP DETECTOR DATA

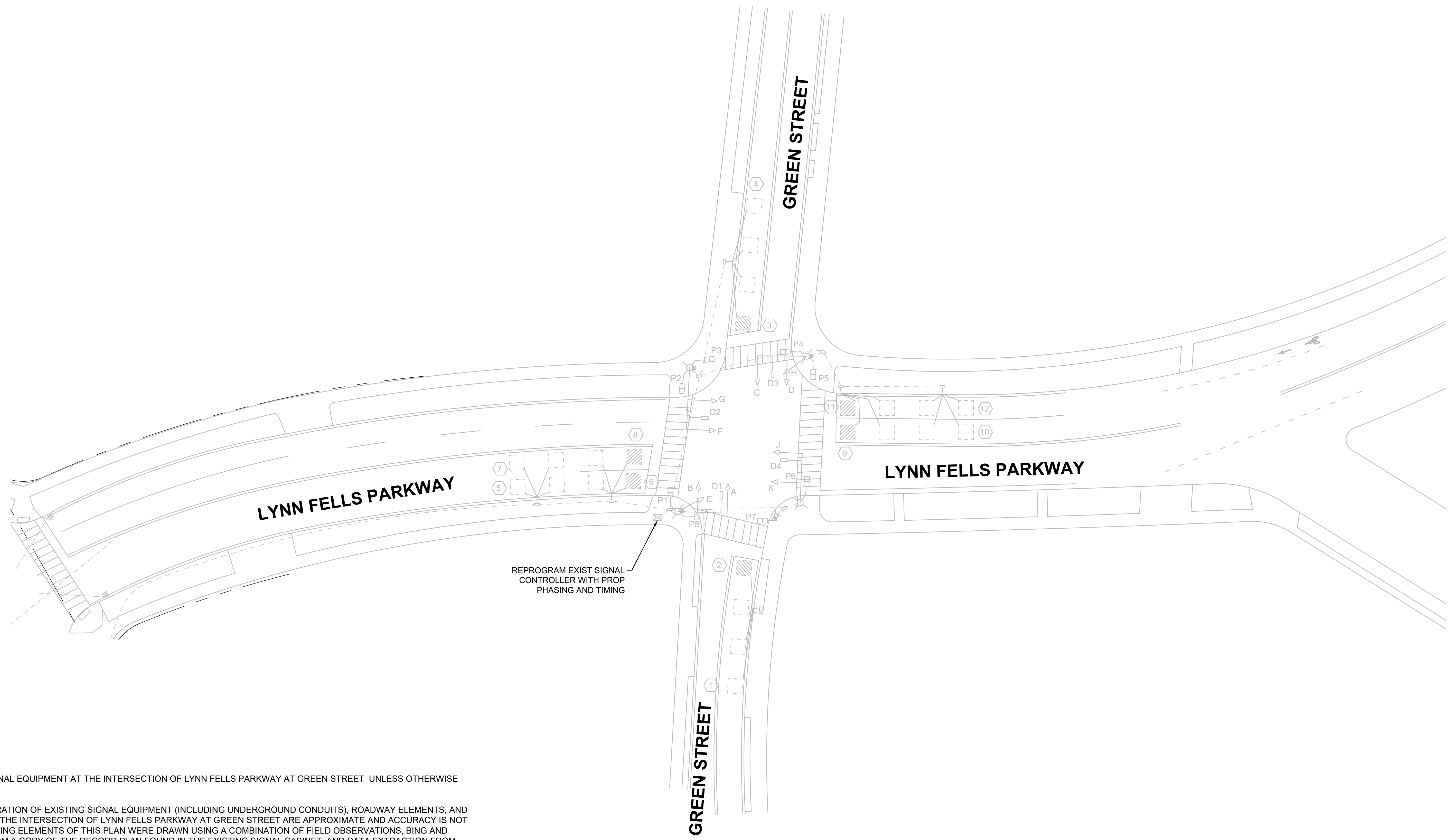
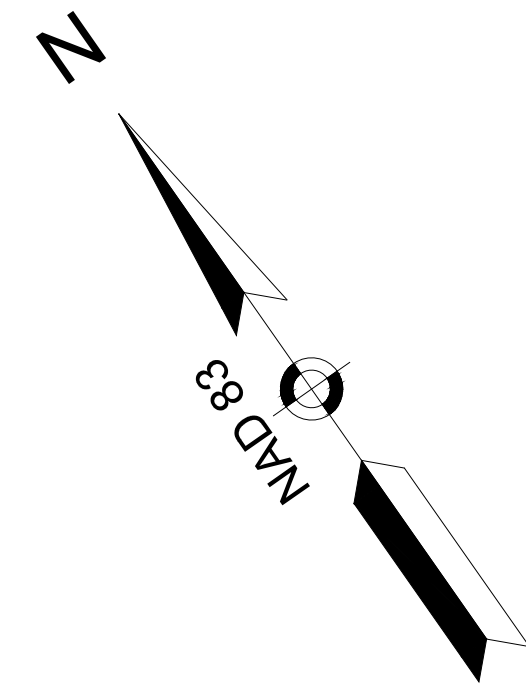
DETECTION GROUP NO.	QTY.	SIZE	NUM. OF TURNS	Ø CALLED	Ø EXT.	MODE A=PULSE B=PRES.	DELAY TIME	EXT. TIME (SEC)
1	3	6' X 6'	EXIST	4	4	B	-	-
2	2	6' X 6' (BICYCLE)	EXIST	4	4	B	-	-
3	1	6' X 6' (BICYCLE)	EXIST	8	8	B	-	-
4	3	6' X 6'	EXIST	8	8	B	-	-
5	1	6' X 6' (BICYCLE)	EXIST	3	3	B	-	-
6	3	6' X 6'	EXIST	8	3 & 8	B	-	-
7	3	6' X 6'	EXIST	6	6	B	-	-
8	1	6' X 6' (BICYCLE)	EXIST	6	6	B	-	-
9	3	6' X 6'	EXIST	1	1	B	-	-
10	1	6' X 6' (BICYCLE)	EXIST	1	1	B	-	-
11	1	6' X 6' (BICYCLE)	EXIST	2	2	B	-	-
12	3	6' X 6'	EXIST	2	2	B	-	-
13	1	6' X 6' (BICYCLE)	EXIST	2	2	B	-	-
14	3	6' X 6'	EXIST	2	2	B	-	-
15	3	6' X 6'	EXIST	4	4	B	-	-

**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION**

**INTERSECTION IMPROVEMENTS AT
LYNN FELS PARKWAY AND MELROSE STREET
MELROSE**

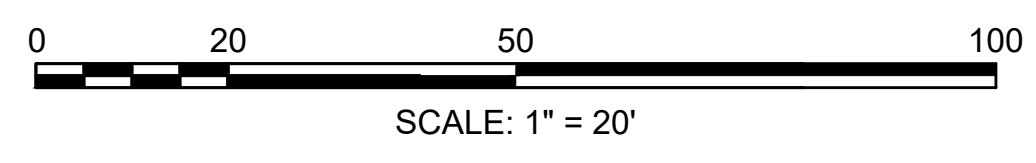
SURVEY BY: NITSCH	TRAFFIC SIGNAL DATA PLAN (LYNN FELS PKWY AT MAIN ST)	SHEET NO.
DRAWN BY: KC		20
CHECKED BY: BG		CONT. P20-3361-D1A
APPROVED BY: JM	SCALE: NTS	DATE: 4/11/2023
		20 OF 33

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TRAFFIC SIGNAL NOTES:

1. RETAIN ALL EXISTING SIGNAL EQUIPMENT AT THE INTERSECTION OF LYNN FELLS PARKWAY AT GREEN STREET UNLESS OTHERWISE NOTED.
2. LOCATION AND CONFIGURATION OF EXISTING SIGNAL EQUIPMENT (INCLUDING UNDERGROUND CONDUITS), ROADWAY ELEMENTS, AND PAVEMENT MARKINGS AT THE INTERSECTION OF LYNN FELLS PARKWAY AT GREEN STREET ARE APPROXIMATE AND ACCURACY IS NOT GUARANTEED. THE EXISTING ELEMENTS OF THIS PLAN WERE DRAWN USING A COMBINATION OF FIELD OBSERVATIONS, BING AND GOOGLE MAPS, DATA FROM A COPY OF THE RECORD PLAN FOUND IN THE EXISTING SIGNAL CABINET, AND DATA EXTRACTION FROM THE EXISTING SIGNAL CONTROLLER.
3. SIGNAL PHASE NUMBERING HAS BEEN CHANGED RELATIVE TO EXISTING.
4. LOOP DETECTOR GROUP NUMBERS ON THIS PLAN MATCH THOSE ON THE RECORD PLAN, AND THESE NUMBERS MAY NOT MATCH THE EXISTING CONFIGURATION.
5. CONSTRUCTION OF THE TRAFFIC CONTROL SIGNAL SYSTEM SHALL CONFORM TO THE MASSACHUSETTS DEPARTMENT OF TRANSPORTATION'S (MASSDOT) "STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES", 2022 EDITION AS AMENDED, AND THE 2009 FEDERAL HIGHWAY ADMINISTRATION'S "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", (MUTCD) LATEST EDITION AS AMENDED.
6. SEE SHEET 2 - 3 FOR LEGENDS, ABBREVIATIONS, AND GENERAL NOTES.
7. SEE SHEET XX - XX FOR TRAFFIC SIGNAL COORDINATION PLAN.



COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION DIVISION OF ENGINEERING AND DIVISION OF PLANNING AND RESOURCE PROTECTION		
INTERSECTION IMPROVEMENTS AT LYNN FELLS PARKWAY AND MELROSE STREET MELROSE		
SURVEY BY: NTSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	TRAFFIC SIGNAL PLAN (LYNN FELLS PKWY AT GREEN ST)	SHEET NO. 21 21 OF 33
CONT. P20-3361-D1A		SCALE: 1 IN. = 20 FT. DATE: 4/11/2023

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TRAFFIC MANAGEMENT NOTES:

1. ALL TRAFFIC MANAGEMENT AND WORK ZONE TRAFFIC CONTROL MEASURES SHALL CONFORM TO THE REQUIREMENTS OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) CURRENT EDITION WITH MASSACHUSETTS AMMENDMENTS, THE STANDARD SPECIFICATIONS, THE PROJECT SPECIAL PROVISIONS, AND THE FOLLOWING NOTES.
2. ALL SIGN LEGENDS, BORDERS, AND MOUNTING SHALL BE IN ACCORDANCE WITH THE MUTCD.
3. THE TRAFFIC MANAGEMENT PLANS CONTAINED HEREIN ARE GIVEN AS A GUIDE FOR TYPICAL WORK ZONE TRAFFIC CONTROL APPLICATIONS FOR THE TYPES OF WORK ANTICIPATED FOR THIS PROJECT. THEY ARE NOT INTENDED TO COVER ALL POSSIBLE CONSTRUCTION OPERATIONS WHICH THE CONTRACTOR MAY CHOOSE TO EMPLOY. WORK ZONE TRAFFIC CONTROL FOR OTHER CONSTRUCTION OPERATIONS OR OTHER TRAFFIC SITUATIONS IF APPLICABLE SHALL BE IN ACCORDANCE WITH THE MUTCD AND AS APPROVED OR DIRECTED BY THE ENGINEER.
4. NO CONSTRUCTION VEHICLES SHALL BE PARKED WITHIN THE TRAVEL WAY WITHOUT PROPER PROTECTION AND APPROVAL OF THE ENGINEER.
5. TEMPORARY CONSTRUCTION SIGNING AND ALL OTHER TRAFFIC CONTROL DEVICES SHALL BE IN PLACE PRIOR TO THE START OF ANY WORK.
6. ALL WARNING SIGNS SHALL BE BLACK LEGEND ON A REFLECTIVE ORANGE BACKGROUND AND IN ACCORDANCE WITH THE MUTCD. ALL REGULATORY SIGNS SHALL BE BLACK LEGEND ON A WHITE REFLECTIVE BACKGROUND. ALL CONSTRUCTION SIGNS SHALL BE ATTACHED TO THEIR OWN INDEPENDENT SUPPORTS UNLESS SHOWN OTHERWISE.
7. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO AVOID PLACING TEMPORARY TRAFFIC CONTROL DEVICES ON PRIVATE PROPERTY. IF SUCH PLACEMENT ON PRIVATE PROPERTY IS UNAVOIDABLE, IT SHALL BE DONE WITH THE EXPLICIT APPROVAL OF THE PROPERTY OWNER AND THE ENGINEER.
8. ABUTTER ACCESS SHALL NOT BE CLOSED EXCEPT FOR SHORT PERIODS AND ONLY WITH THE APPROVAL OF THE ENGINEER. THE CONTRACTOR SHALL NOTIFY EACH ABUTTER AT LEAST 24 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OF ACCESS, SUCH AS CONDUIT INSTALLATION, EXISTING PAVEMENT EXCAVATION, TEMPORARY DRIVEWAY PAVEMENT PLACEMENT AND SIMILAR OPERATIONS.
9. THE CONTRACTOR SHALL PROVIDE IMMEDIATE ACCESS TO EMERGENCY VEHICLES AT ALL TIMES.
10. GRADE DIFFERENCES IN EXCESS OF 2" DURING NON-WORKING HOURS WILL REQUIRE DELINEATION BY USE OF DRUMS.
11. GRADE DIFFERENCES IN EXCESS OF 4" DURING NON-WORKING HOURS SHALL BE PROTECTED BY BACKFILLING WITH A TRANSITION OF GRAVEL OR OTHER MATERIAL TO BE COMPACTED AT A 4:1 SLOPE, AND DELINEATED BY DRUMS.
12. CONSTRUCTION SIGNS NOT APPLICABLE TO VARIOUS STAGES OF CONSTRUCTION SHALL BE REMOVED OR COVERED.
13. USE MA-W20-7b SIGNS ONLY WHEN POLICE OFFICER IS DIRECTING TRAFFIC. THEY SHALL BE TAKEN DOWN OR COVERED AT THE CLOSE OF EACH OPERATION.
14. MAINTAIN PEDESTRIAN ACCESS THROUGH THE WORK AREA AT ALL TIMES. THE POLICE DETAIL SHALL PROVIDE CONTROL TO CROSS PEDESTRIANS ON ROADWAY TO SIDEWALK. PROVIDE TEMPORARY CROSSWALKS AND RAMPS AS NEEDED AND AS DIRECTED BY THE ENGINEER.
15. ALL CONSTRUCTION SIGNING AND OTHER TRAFFIC MAINTENANCE DEVICES SHALL CONFORM WITH THE 2009 MUTCD AS AMENDED, MASH, AND MASSDOT STANDARDS.
16. ADVANCE WARNING SIGNS NO LONGER APPLICABLE, WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS, SHALL EITHER BE COVERED OR REMOVED AS SOON AS POSSIBLE. NO SIGN SHALL BE VISIBLE TO TRAFFIC THAT MAY CONFLICT WITH ACTUAL ROADWAY CONDITIONS.
17. ALL DISTANCES MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. HOWEVER, MINIMUM DISTANCES, WHERE INDICATED, SHOULD BE MAINTAINED.
18. THE CONTRACTOR SHALL USE TEMPORARY PATCHING OR BEVELED STEEL PLATES TO COVER PIPE TRENCHES AND OTHER EXCAVATED HOLES NOT COMPLETED BY THE END OF EACH WORK DAY.
19. ALL DRUMS WITH FLASHERS, SIGN SUPPORTS LOCATED ON OR NEAR THE TRAVELED WAY, CHANNELIZING DEVICES, BARRIERS, AND CRASH ATTENUATORS MUST PASS THE CRITERIA SET FORTH IN NCHRP REPORT 350, "RECOMMENDED PROCEDURES FOR THE SAFETY PERFORMANCE EVALUATION OF HIGHWAY FEATURES" AND/OR "MANUAL FOR ASSESSING SAFETY HARDWARE" (MASH).
20. MINIMUM LANE WIDTH IS TO BE 11 FEET UNLESS OTHERWISE SHOWN. MINIMUM LANE WIDTH WILL BE MEASURED FROM THE EDGE OF DRUMS OR CONES.
21. ORANGE CONSTRUCTION FLAGS MAY BE USED ON ADVANCE WARNING SIGNS AS DIRECTED BY THE ENGINEER. FLAGS SHALL BE A MINIMUM OF 16" X 16".
22. MAINTAIN EXISTING PAVEMENT MARKINGS WHERE APPLICABLE. WHEN LANES SHIFT, IF NECESSARY, EXISTING MARKINGS SHALL BE REMOVED AND TEMPORARY PAVEMENT MARKING SHALL BE PROVIDED.
23. AT THE END OF EACH WORK DAY, NO TRAFFIC CONTROL DEVICES SHALL REMAIN IN THE ROADWAY AND ALL LANES SHALL BE OPEN FOR TRAFFIC FLOW.
24. THE CONTRACTOR MAY PROPOSE TO USE A DIFFERENT SEQUENCE OF WORK AREAS THAN WHAT IS BEING PROPOSED IN THESE DOCUMENTS. THE CONTRACTOR SHALL SUBMIT PHASING AND TRAFFIC MANAGEMENT PLANS FOR APPROVAL BY THE ENGINEER.
25. MAXIMUM SPACING OF CHANNELIZING DEVICES IN A TAPER IS EQUAL IN FEET TO THE SPEED LIMIT IN MPH. (20' SPACING TYPICAL ON TAPER, 30' SPACING TYPICAL ON TANGENTS.)
26. CHANNELIZATION WILL BE ACCOMPLISHED THROUGH THE USE OF REFLECTORIZED PLASTIC DRUMS OR CONES.
27. THE FIRST TEN PLASTIC DRUMS OF A TAPER SHALL HAVE SEQUENTIAL FLASHING LIGHTS.
28. POLICE DETAIL SHALL BE USED WHILE SETTING UP THE TEMPORARY TRAFFIC CONTROL DEVICES ON THE ROADWAY.
29. EACH WORK ZONE SHALL HAVE MA-R2-10a, MA-R2-10e, AND W-20 SERIES SIGNS WHERE APPLICABLE.
30. POLICE DETAILS SHALL BE EMPLOYED AND SHALL BE SUBSTITUTED WITH CERTIFIED ROADWAY FLAGGERS AS DIRECTED BY THE ENGINEER AND PER SECTION 850 "TRAFFIC CONTROLS FOR CONSTRUCTION AND MAINTENANCE OPERATIONS" IN THE MASSDOT STANDARD SPECIFICATIONS.
31. ALL DRIVEWAYS AND STREETS SHALL REMAIN OPEN AT ALL TIMES EXCEPT FOR SHORT PERIODS AS APPROVED BY THE ENGINEER.

SUGGESTED WORK ZONE WARNING SIGN SPACING

ROAD TYPE	DISTANCE BETWEEN SIGNS **		
	A	B	C
LOCAL OR LOW VOLUME ROADWAYS*	350 (100)	350 (100)	350 (100)
MOST OTHER ROADWAYS*	500 (150)	500 (150)	500 (150)
FREEWAYS AND EXPRESSWAYS*	1,000 (300)	1,500 (450)	2,640 (800)

* ROAD TYPE TO BE DETERMINED BY MASSDOT OFFICE OF TRANSPORTATION PLANNING.

** DISTANCES ARE SHOWN IN FEET (METERS). THE COLUMN HEADINGS A, B, AND C ARE THE DIMENSIONS SHOWN IN THE DETAIL/ TYPICAL SETUP FIGURES. THE A DIMENSION IS THE DISTANCE FROM THE TRANSITION OR POINT OF RESTRICTION TO THE FIRST SIGN. THE B DIMENSION IS THE DISTANCE BETWEEN THE FIRST AND SECOND SIGNS. THE C DIMENSION IS THE DISTANCE BETWEEN THE SECOND AND THIRD SIGNS. (THE "THIRD" SIGN IS THE FIRST ONE TYPICALLY ENCOUNTERED BY A DRIVER APPROACHING A TEMPORARY TRAFFIC CONTROL (TTC) ZONE.)

THE "THIRD" SIGN ABOVE IS TYPICALLY REFERRED TO AS AN "ADVANCE WARNING" SIGN ON THE TTCP SETUPS. THESE ADVANCE WARNING SIGNS ARE LOCATED PRIOR TO THE PROJECT LIMITS ON ALL APPROACHES (i.e. THE W20-1 SERIES (ROAD WORK XX FT) SIGNS), AND USUALLY REMAIN FOR THE DURATION OF THE PROJECT. ADDITIONAL SIGNS (i.e. "RIGHT LANE CLOSED 1 MILE" AND "LEFT LANE CLOSED 1 MILE") HAVE BEEN SHOWN IN SOME FIGURES AS EXAMPLES OF REINFORCEMENT SIGN PLACEMENT BUT ARE USED IN RARE OCCASIONS.

THE FIRST AND SECOND WARNING SIGNS ABOVE ARE REFERRED TO AS THE OPERATIONAL (DAY-TO-DAY) WORK ZONE SIGNS AND MAY BE MOVED DEPENDING ON WHERE THE SPECIFIC ROADWAY WORK FOR THAT DAY IS LOCATED.

MA-R2-10a SIGNS SHALL BE PLACED BETWEEN THE SECOND AND THIRD SIGNS AS DESCRIBED ABOVE.

MA-R2-10a, MA-R2-10e, AND W20-1 SERIES SIGNS ARE TO BE INCLUDED ON ALL DETAILS/TYPICAL SETUPS.

TAPER LENGTH CRITERIA FOR TEMPORARY TRAFFIC CONTROL ZONES

TYPE OF TAPER	TAPER LENGTH (L)*
MERGING TAPER	AT LEAST L
SHIFTING TAPER	AT LEAST 0.5L
SHOULDER TAPER	AT LEAST 0.33L
ONE-LANE, TWO-WAY TRAFFIC TAPER	50 FT MIN.(15 m) 100 FT(30 m) MAX.
DOWNSTREAM TAPER	50 FT MIN.(15 m) 100 FT MAX.(30 m) PER LANE

FORMULAS FOR DETERMINING TAPER LENGTHS

SPEED LIMIT (S)	TAPER LENGTH (L) FEET
40 MPH OR LESS	$L = \frac{WS^2}{60}$
45 MPH OR MORE	$L = WS$

WHERE: L = TAPER LENGTH IN FEET

W = WIDTH OF OFFSET IN FEET

S = POSTED SPEED LIMIT, OR OFF-PEAK 85TH-PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED IN MPH (KM/H)

STOPPING SIGHT DISTANCE AS A FUNCTION OF SPEED

SPEED* (mph)	DISTANCE (ft)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645
70	730
75	820

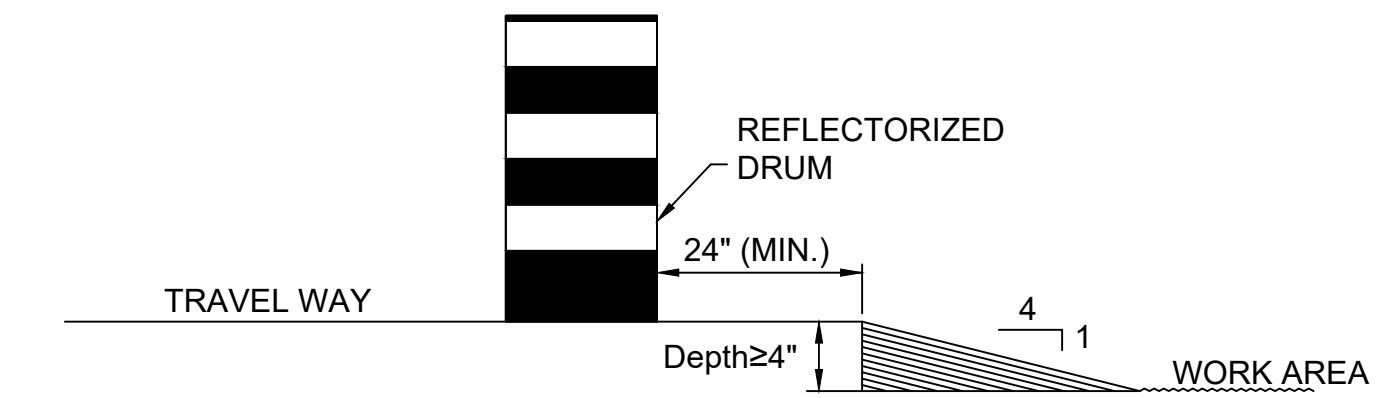
*POSTED SPEED, OFF-PEAK 85TH-PERCENTILE SPEED PRIOR TO WORK STARTING, OR THE ANTICIPATED OPERATING SPEED

THESE VALUES MAY BE USED TO DETERMINE THE LENGTH OF LONGITUDINAL BUFFER SPACES.

THE DISTANCES IN THE ABOVE CHART REPRESENT THE MINIMAL VALUES FOR BUFFER SPACING.

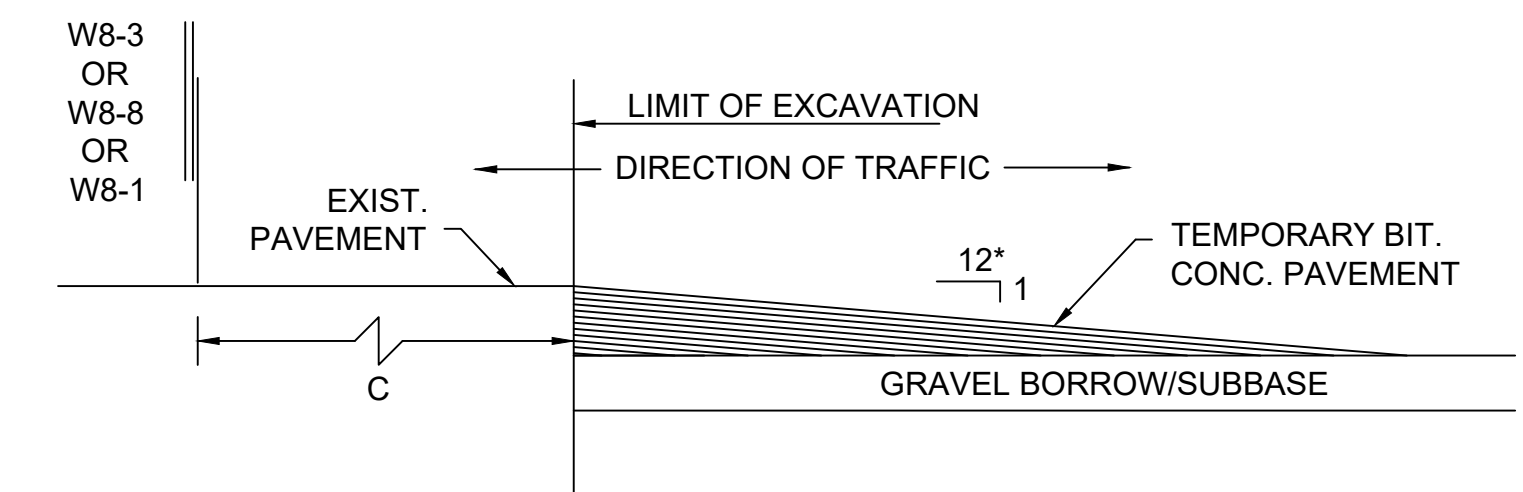
LEGEND:

- REFLECTORIZED PLASTIC DRUM OR 36" CONE
- P/F POLICE/FLAGGER DETAIL
- ▨ TYPE III BARRICADE
- CHANGEABLE MESSAGE SIGN
- ➡ ARROW BOARD
- ▨ WORK ZONE
- ➡ DIRECTION OF TRAFFIC
- ▨ IMPACT ATTENUATOR
- ▨ MEDIAN BARRIER
- ▨ MEDIAN BARRIER WITH WARNING LIGHTS
- ▨ WORK VEHICLE
- ▨ TRUCK MOUNTED ATTENUATOR
- ➡ TRAFFIC OR PEDESTRIAN SIGNAL
- SIGN



LATERAL DROP-OFF DETAIL

NOT TO SCALE



LONGITUDINAL DROP-OFF DETAIL

NOT TO SCALE

* - INCREASE SLOPE RATIO FOR HIGHER SPEEDS

LATERAL AND LONGITUDINAL DROP-OFF DETAILS

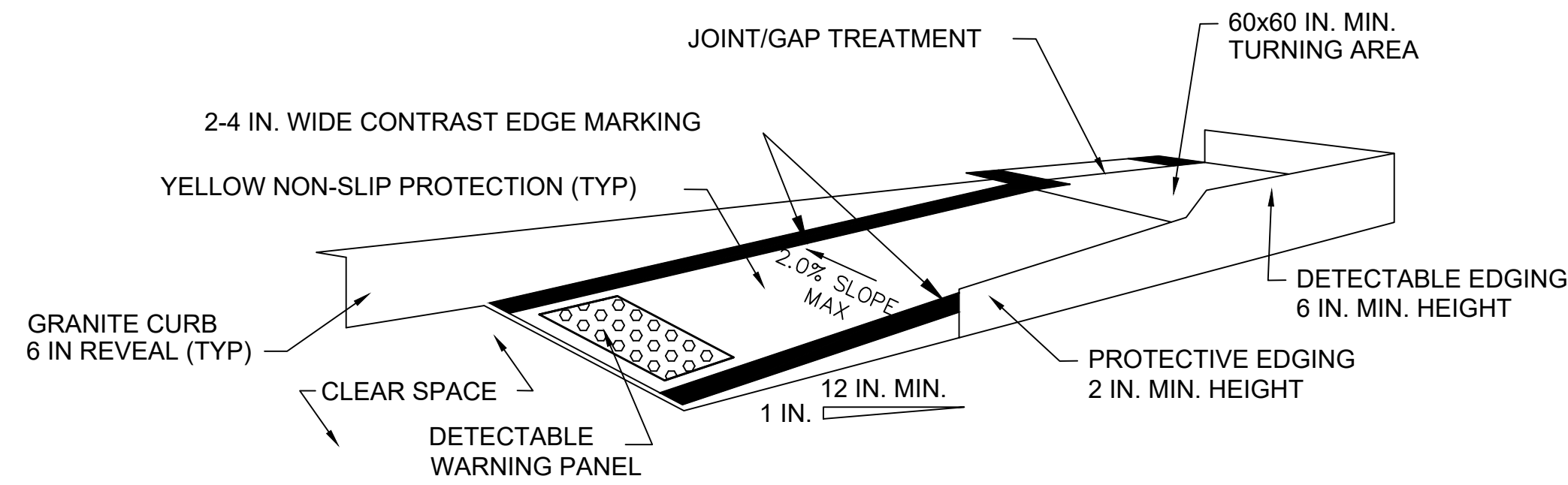
**COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION**

**INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE**

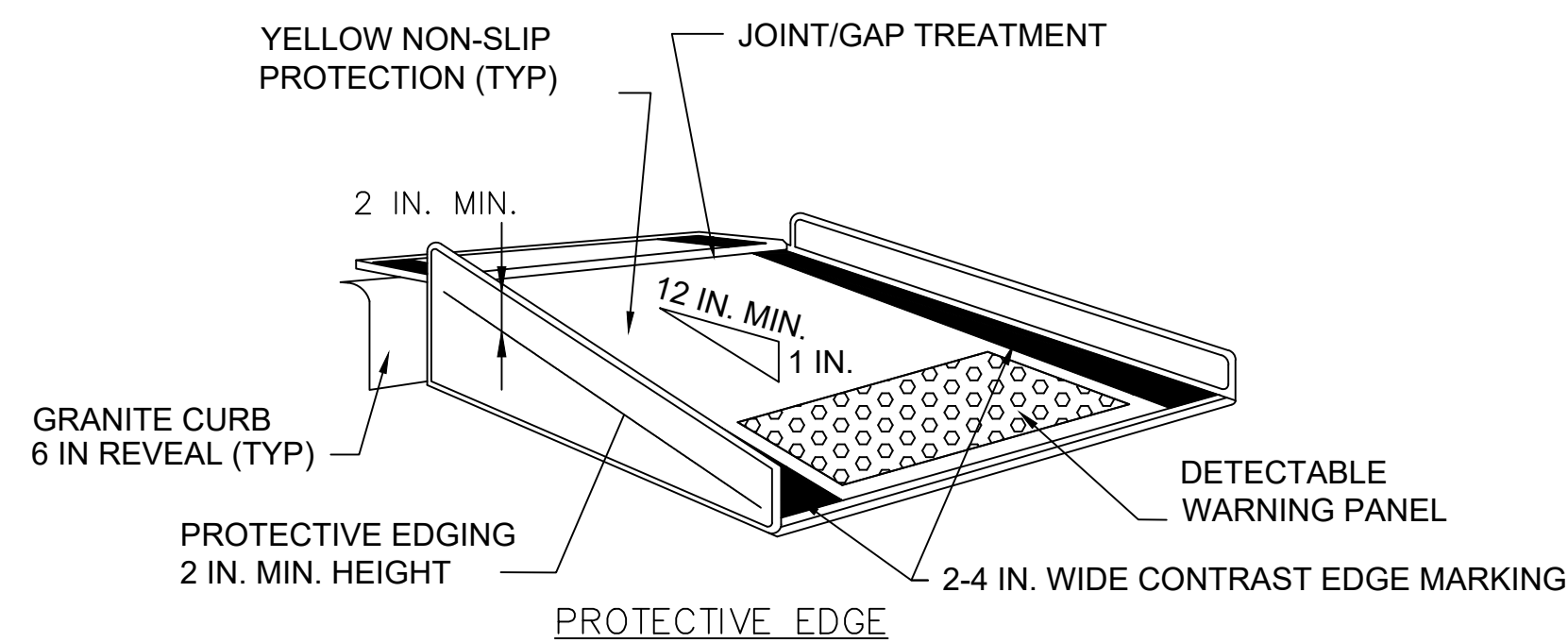
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DRAWN BY: KC		23
CHECKED BY: BG		CONT. P20-3361-D1A
APPROVED BY: JM		DATE: 4/11/2023
		23 OF 33

12589.9_HD(TTCP).DWG Plotted on 3-Aug-2023 8:56 AM

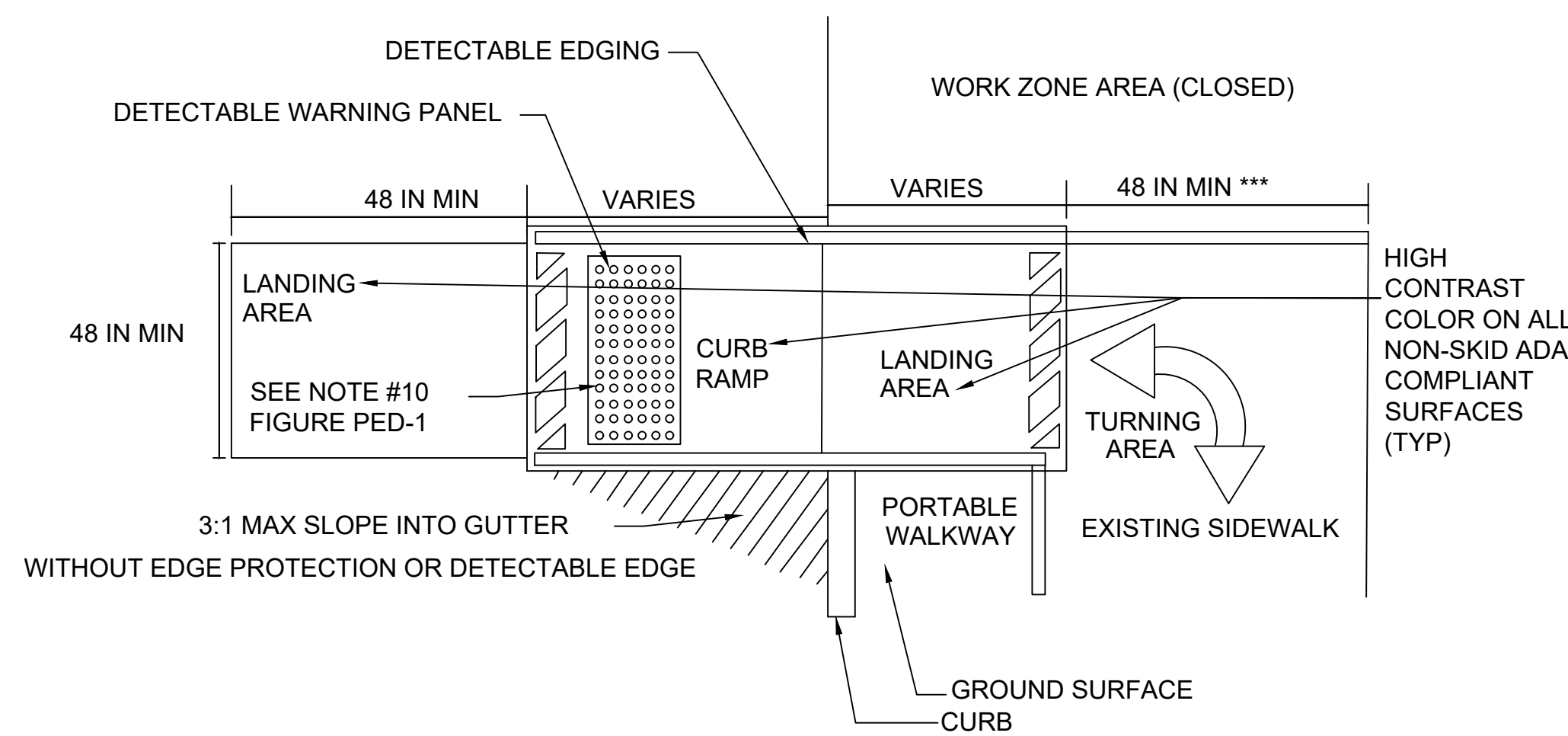
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TEMPORARY CURB RAMP-PARALLEL TO CURB



TEMPORARY CURB RAMP-PERPENDICULAR TO CURB

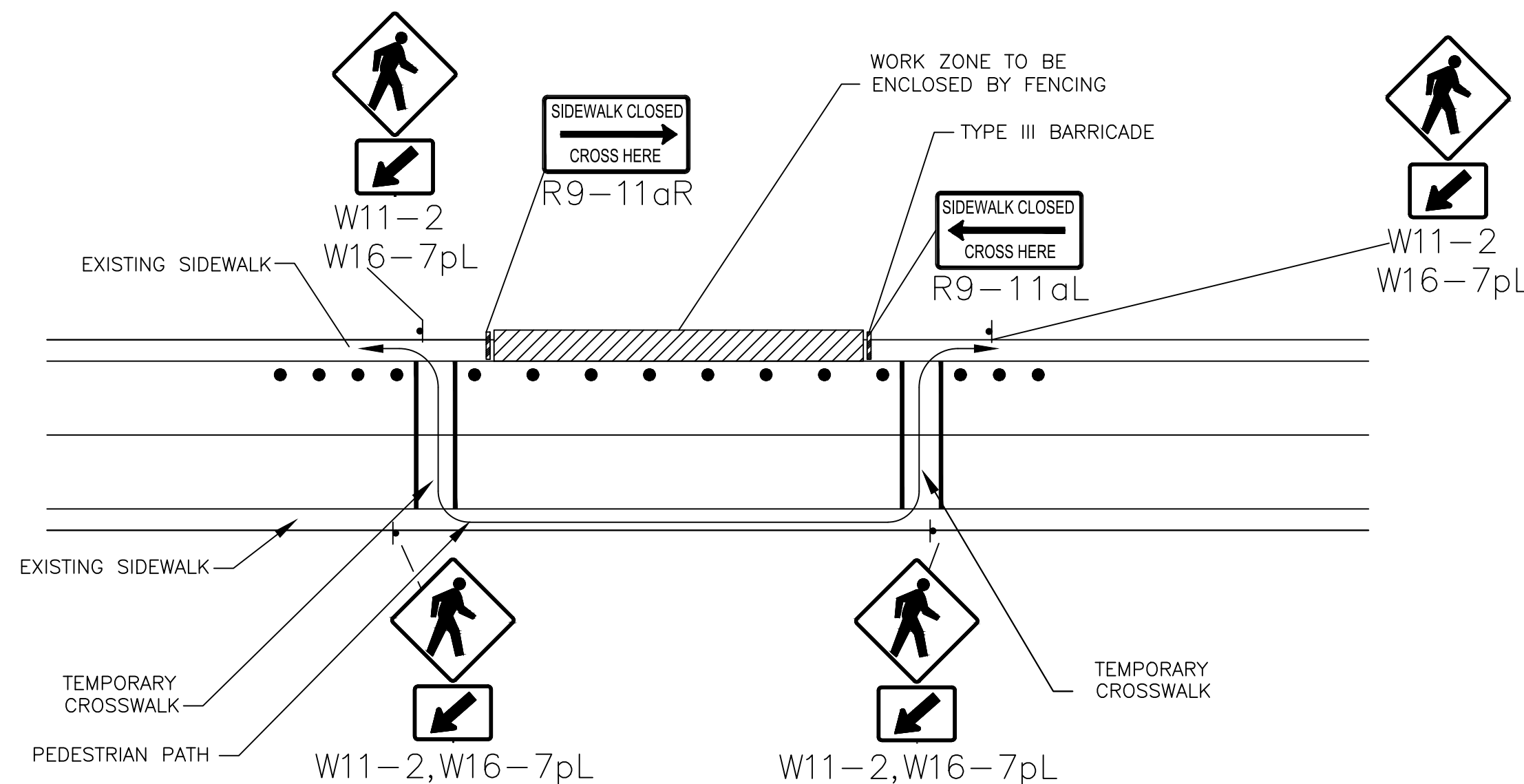


TEMPORARY CURB RAMP

- * -LANDING AREA USED TO OVERLAP NON-ADA COMPLIANT SURFACES.
- ** -DETECTABLE EDGE REMOVED IF A CONTINUOUS SIDEWALK.
- *** -60 IN. IF AN OBSTRUCTION IS AT BACK OF SIDEWALK

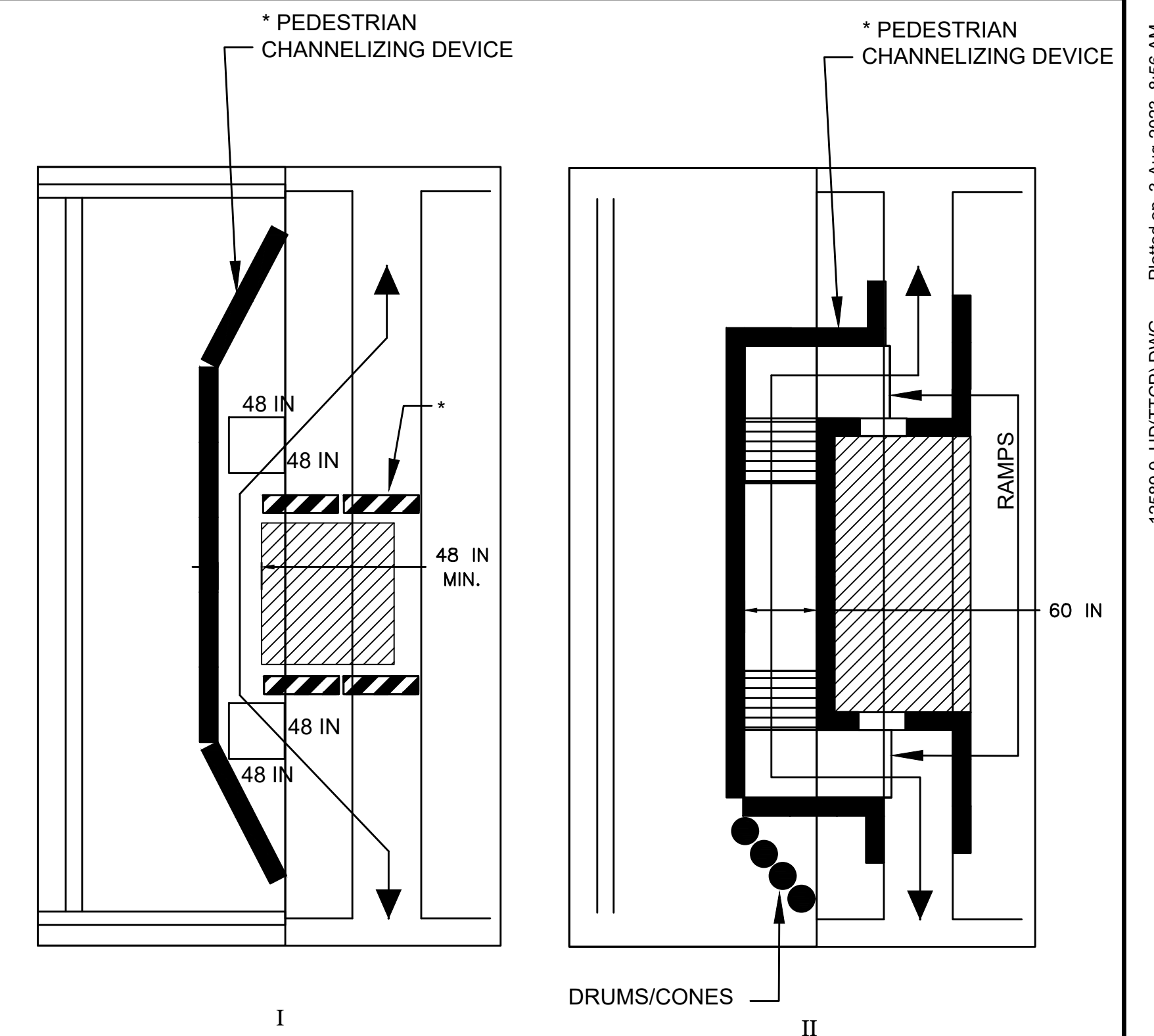
NOTES:

1. CURB RAMPS SHALL BE 60 IN. MINIMUM WIDTH WITH A FIRM, STABLE AND NON-SLIP SURFACE.
2. PROTECTIVE EDGING WITH A 2 IN. MINIMUM HEIGHT SHALL BE INSTALLED WHEN THE CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6 IN. OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN THE CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3 IN. OR MORE.
3. DETECTABLE EDGING WITH 6 IN. MINIMUM HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
4. THE CURB RAMP WALKWAY AND LANDING AREA SURFACE SHALL BE OF A SOLID CONTINUOUS CONTRASTING COLOR ABUTTING UP TO THE EXISTING SIDEWALK.
5. CURB RAMPS AND LANDINGS SHOULD HAVE A 1:50 (2%) MAX CROSS-SLOPE.
6. CLEAR SPACE OF 48x48 IN. MINIMUM SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
7. WATER FLOW IN THE GUTTER SYSTEM SHALL HAVE MINIMAL RESTRICTION.
8. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 0.5 IN. WIDTH.
9. CHANGES BETWEEN SURFACE HEIGHTS SHOULD NOT EXCEED 0.5 IN. LATERAL EDGES SHOULD BE VERTICAL UP TO 0.25 IN. HIGH, AND BEVELED AT 1:2 BETWEEN 0.25 IN. AND 0.5 IN. HEIGHT.
10. IF A TEMPORARY PEDESTRIAN RAMP LEADS TO A CROSSWALK, THEN A DETECTABLE WARNING PAD MUST BE ADHERED TO THE BASE OF THE RAMP. IF IT LEADS TO A PROTECTED PEDESTRIAN BYPASS THAT DOES NOT CONFLICT WITH VEHICULAR TRAFFIC, THEN A PAD SHALL NOT BE INSTALLED ON THE RAMP.

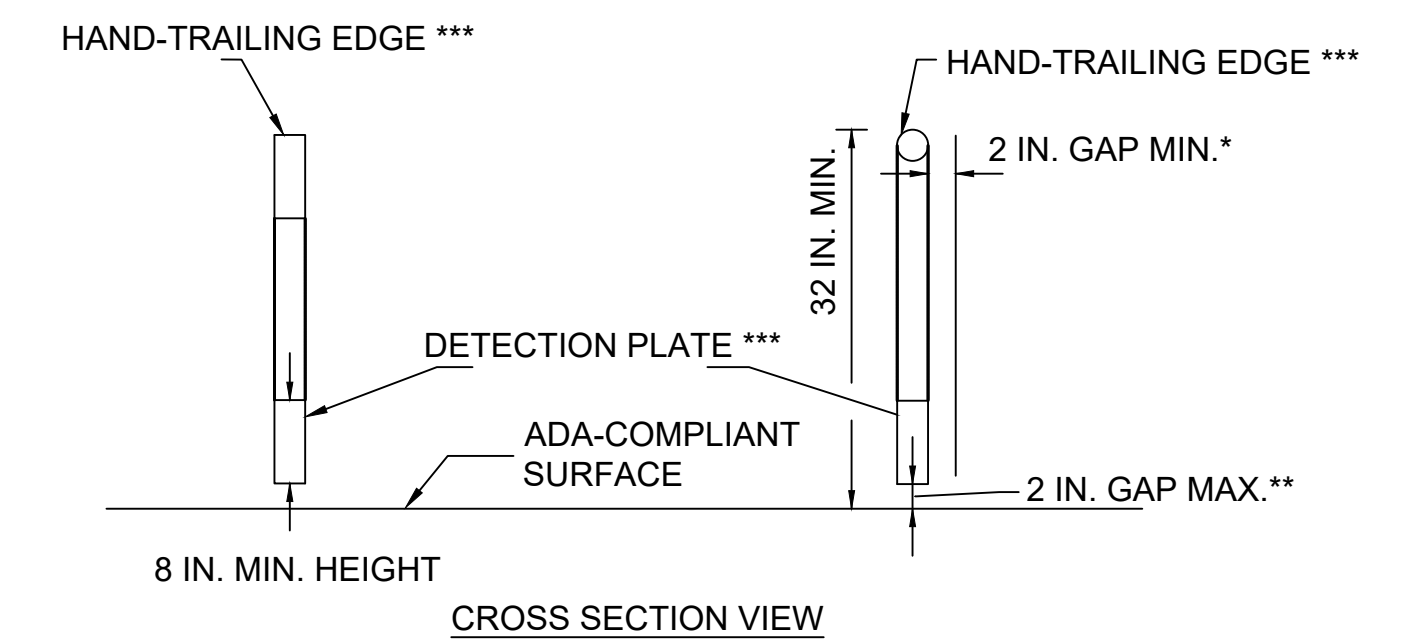


NOTES:

1. ADDITIONAL ADVANCE WARNING MAY BE NECESSARY.
2. CONTROLS ONLY FOR PEDESTRIAN TRAFFIC ARE SHOWN. VEHICULAR TRAFFIC SHOULD BE HANDLED AS SHOWN ELSEWHERE.
3. STREET LIGHTING SHOULD BE CONSIDERED WHEN LOCATING CONTROL DEVICES.
4. IF THE WORK ZONE DOES NOT PERMIT PEDESTRIANS TO TRAVEL ADJACENT TO IT AS SHOWN IN PEDESTRIAN BYPASS TYPE I, TEMPORARY CROSSWALKS WITH APPROPRIATE SIGNS SHOULD BE INSTALLED TO CROSS PEDESTRIANS TO THE OPPOSITE SIDE OF THE STREET AS SHOWN IN PEDESTRIAN BYPASS TYPE II, AND AS DIRECTED BY THE ENGINEER. TEMPORARY CURB RAMPS WILL BE REQUIRED AT ALL TEMPORARY CROSSWALK LOCATIONS.
5. BYPASS IS TO BE USED IN CONJUNCTION WITH THE PROPOSED LANE CLOSURE DETAILS AND DURING CONSTRUCTION STAGING, AS DIRECTED BY THE ENGINEER.
6. THE TEMPORARY SIDEWALK SHOULD BE A MINIMUM OF 4 FEET WIDE. IF THIS WALKWAY EXCEEDS 200 FEET THEN A 5 FOOT X 5 FOOT PASSING ZONE. (FOR SHORT TERM SETUPS < 10 HOURS, THIS CONDITION MAY BE WAIVED. A NOTE WOULD NEED TO BE INCLUDED IN THE TCP THAT STATES HOW THE CONTRACTOR SHOULD ADDRESS THIS ISSUE.)



TEMPORARY PEDESTRIAN DELINEATION DETAILS



PEDESTRIAN CHANNELIZING DEVICE

NOTES:

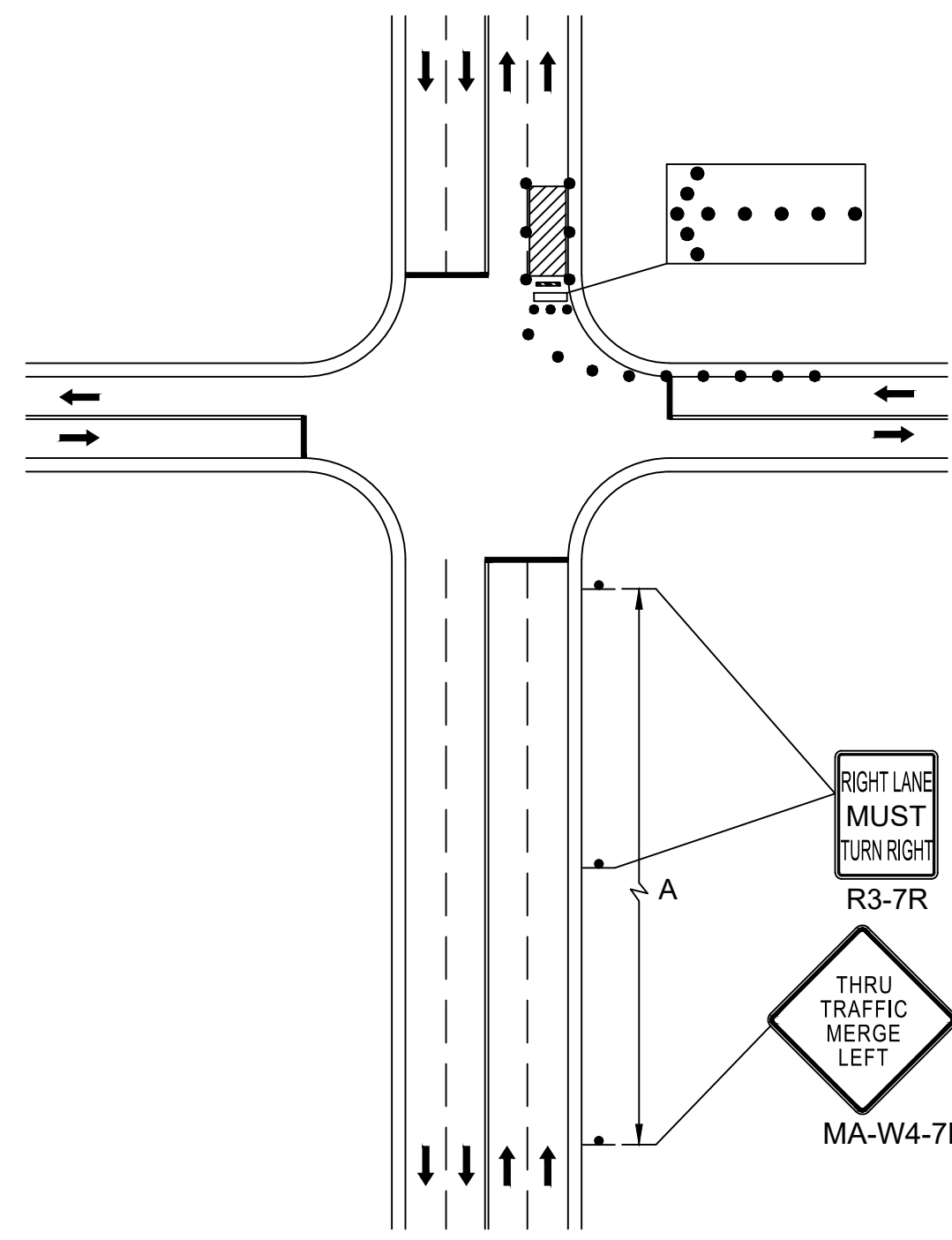
- * THERE SHALL BE A 2 INCH GAP BETWEEN THE HAND-TRAILING EDGE AND ITS SUPPORT.
- ** A MAXIMUM 2 INCH GAP BETWEEN THE BOTTOM OF THE BOTTOM RAIL AND THE SURFACE MAY BE USED TO PROVIDE DRAINAGE.
- *** THE HAND-TRAILING EDGE AND DETECTION PLATE SHALL BE CONTINUOUS THROUGHOUT THE LENGTH OF THE PATH SUCH THAT A PEDESTRIAN USER WITH A LONG CANE CAN FOLLOW IT.

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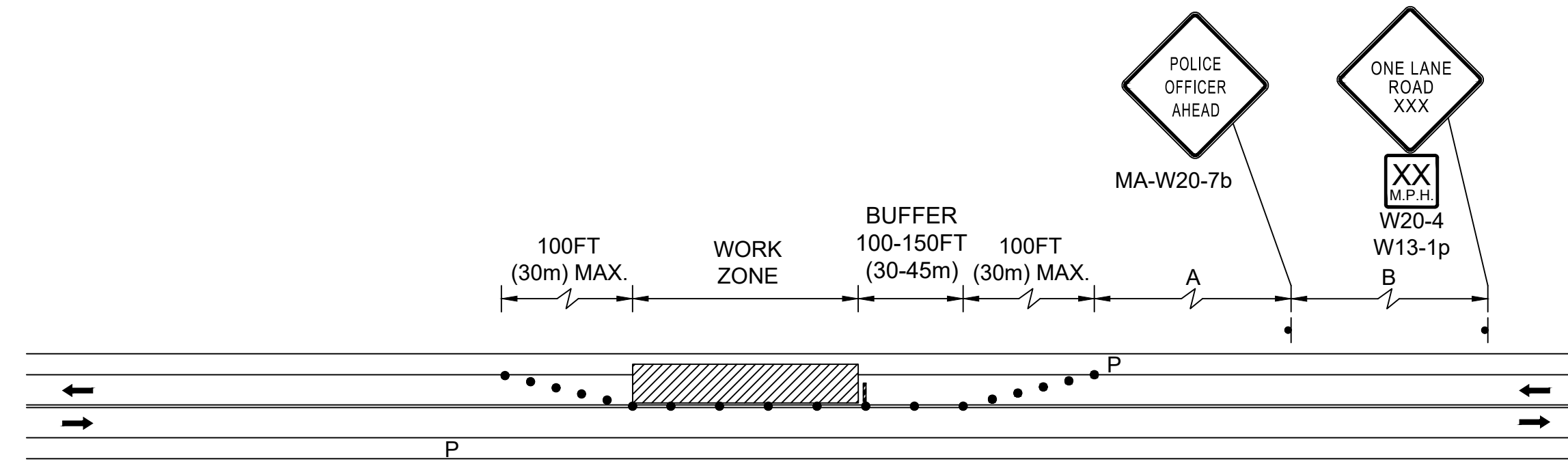
INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

SURVEY BY: NTSCH	TEMPORARY TRAFFIC CONTROL PLAN	SHEET NO.
DRAWN BY: KC		24
CHECKED BY: BG	CONT: P20-3361-D1A	SCALE: NTS
APPROVED BY: JM		DATE: 4/11/2023
		24 OF 33

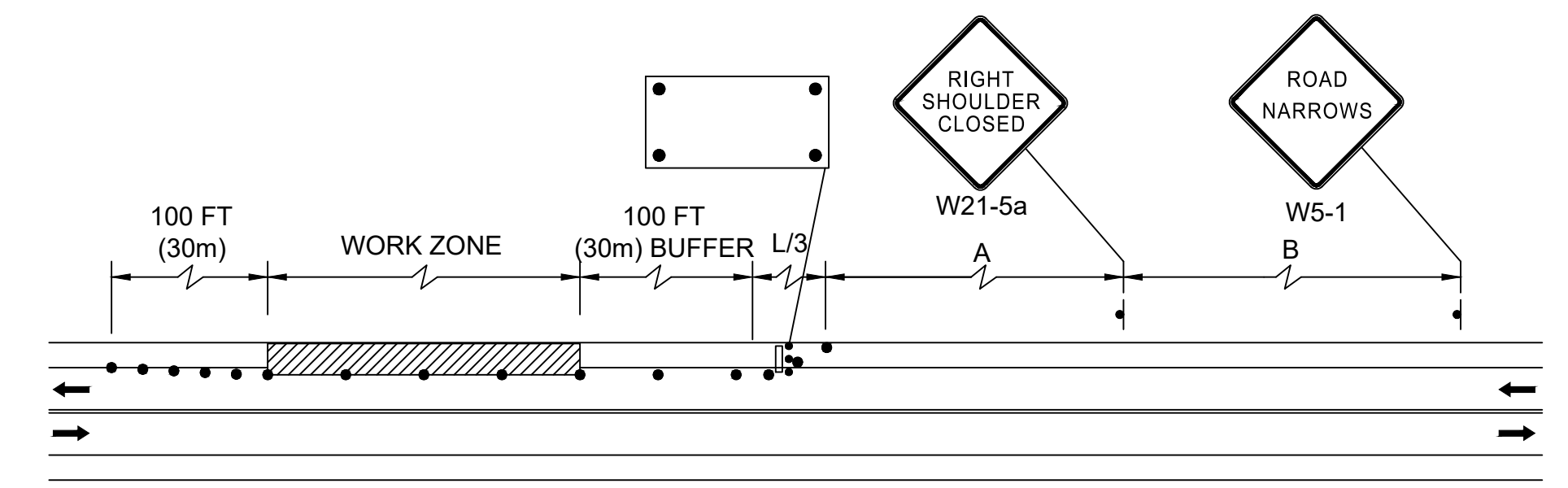
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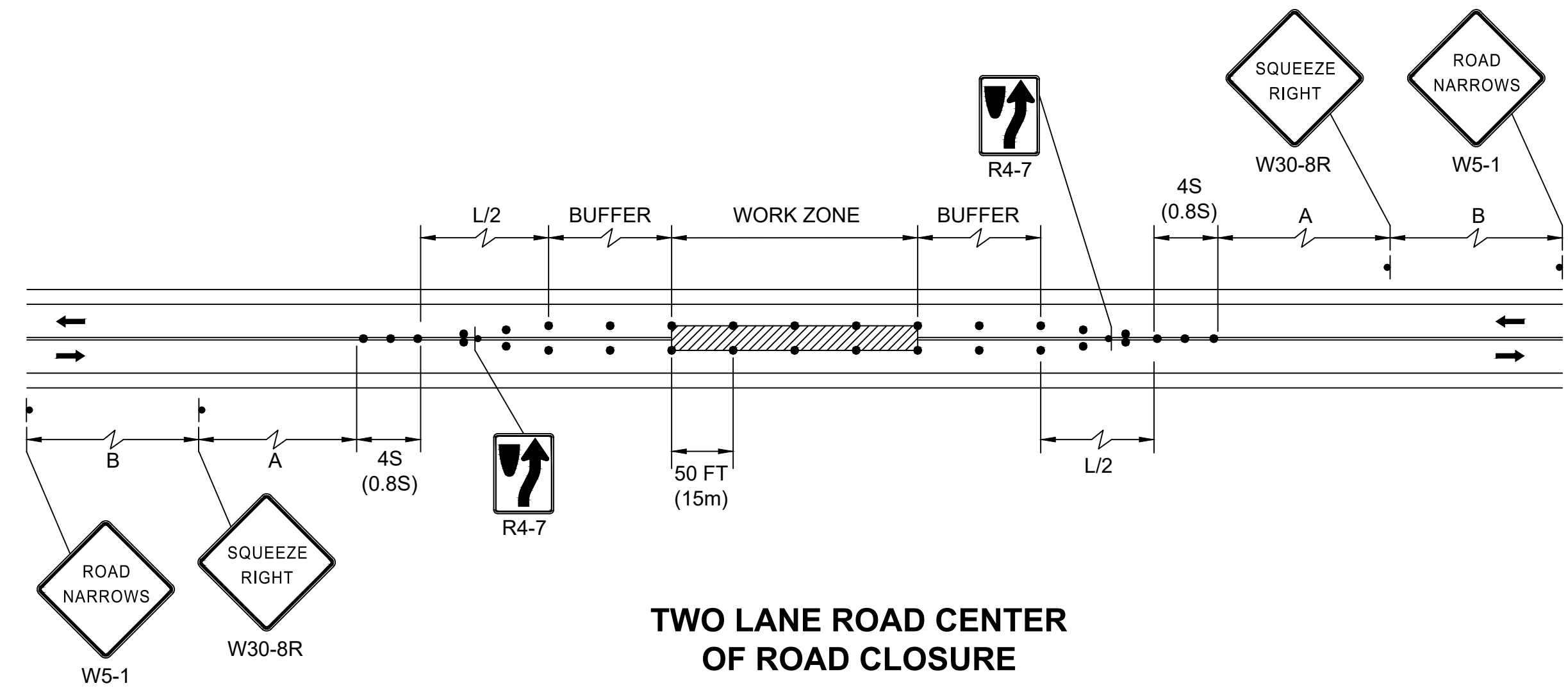
DOUBLE LANE APPROACH FAR SIDE CLOSURE RIGHT LANE



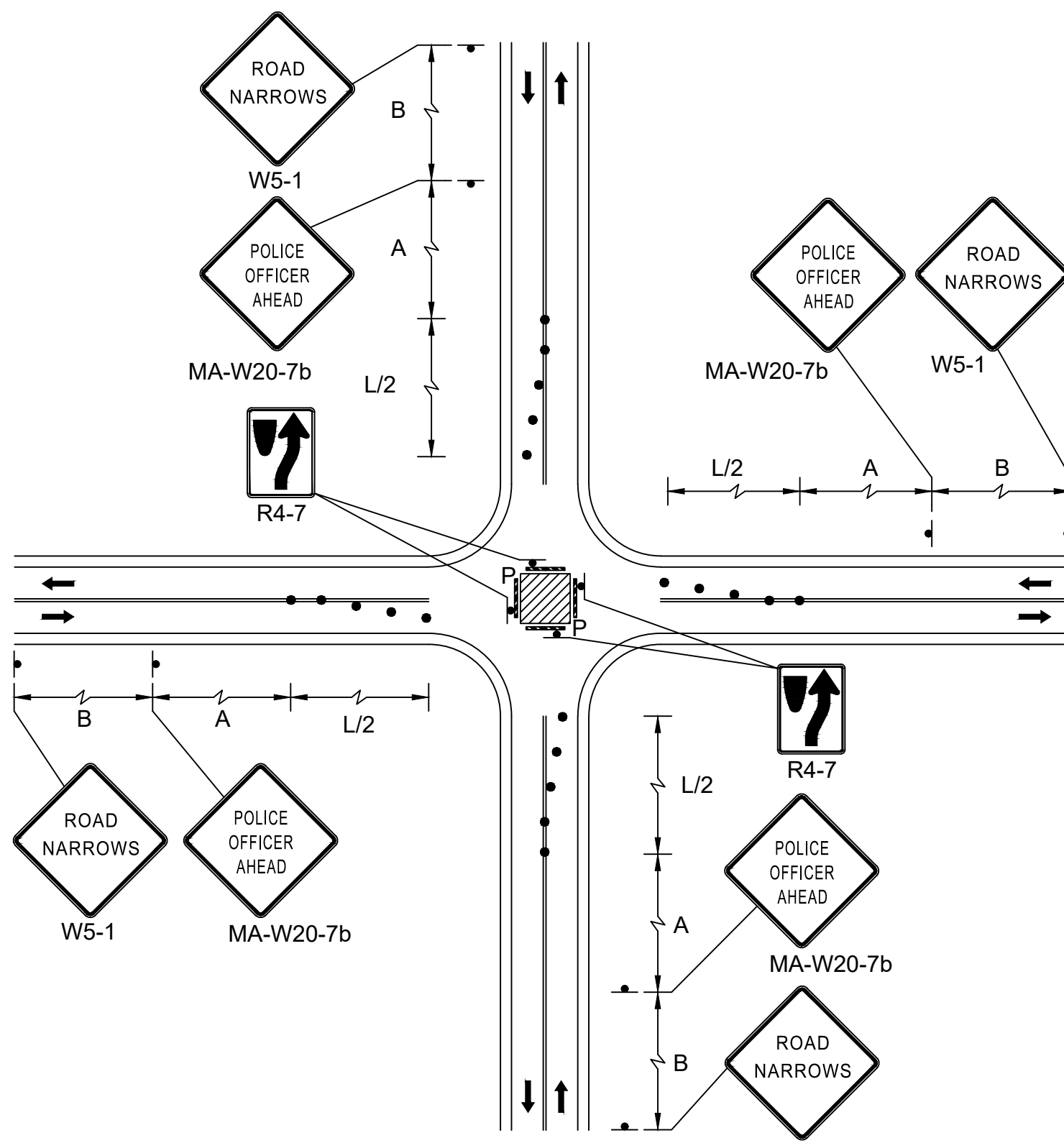
TWO LANE ROAD ALTERNATING TRAFFIC



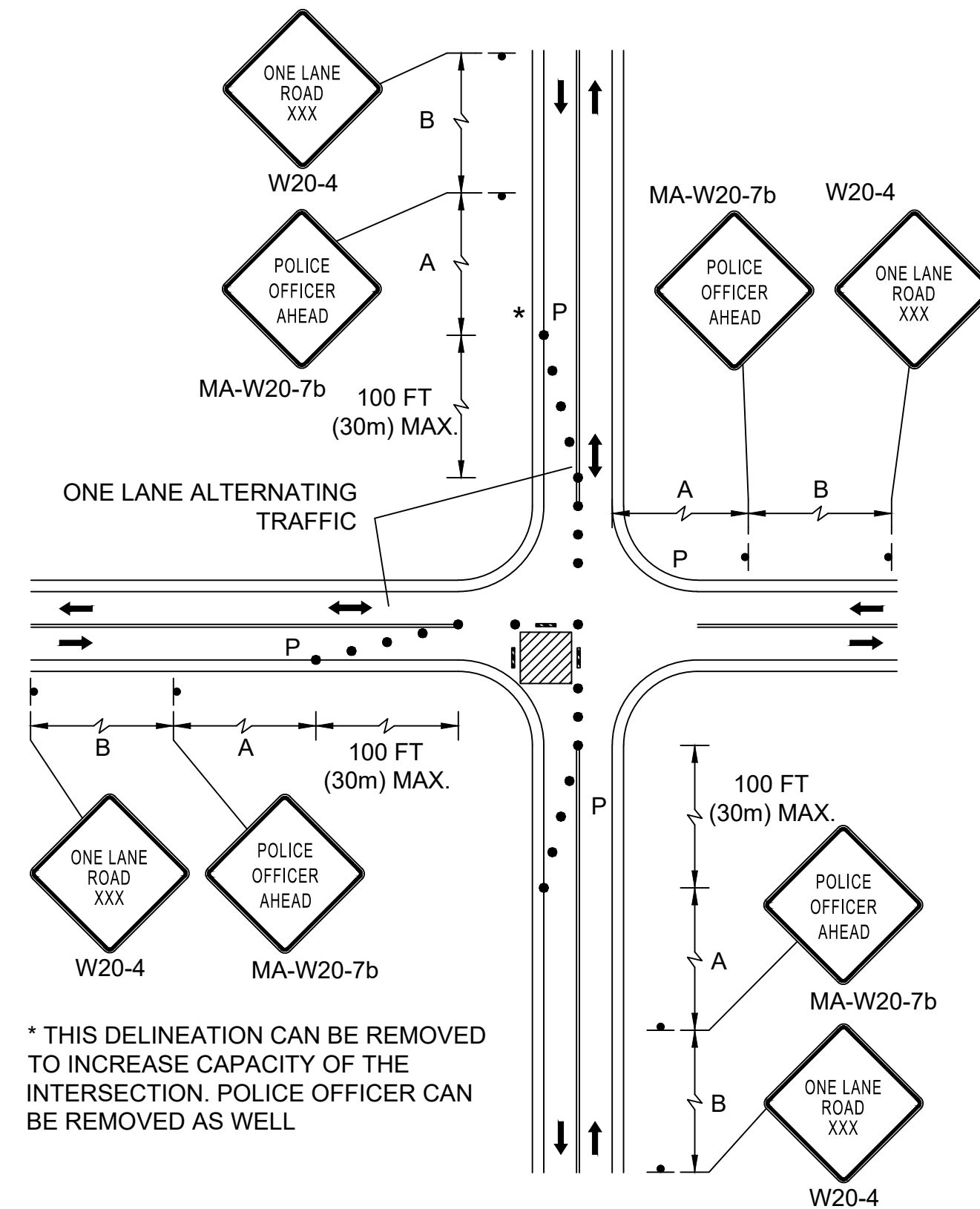
TWO LANE ROAD SHOULDER CLOSURE



TWO LANE ROAD CENTER OF ROAD CLOSURE

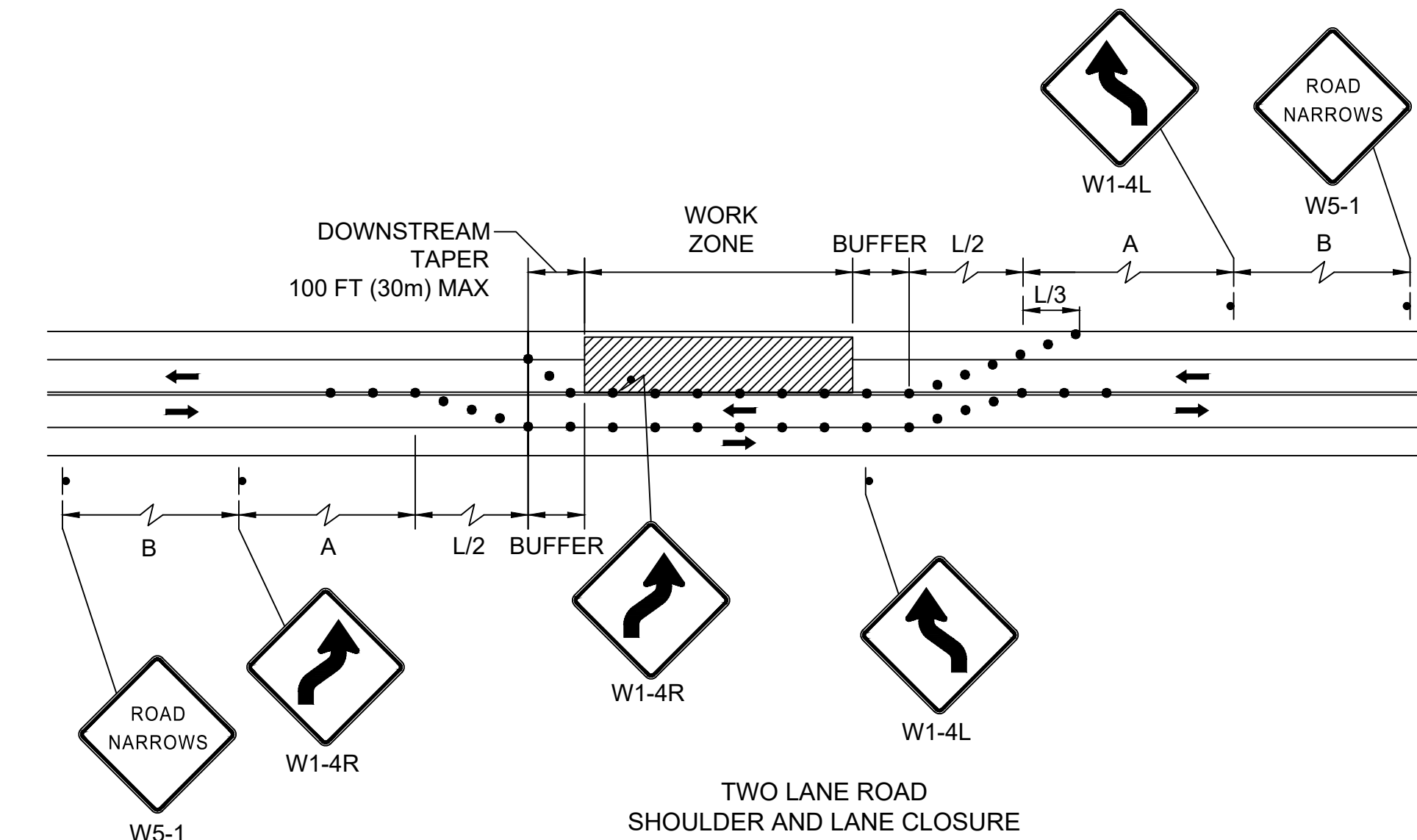


SINGLE LANE APPROACH INTERSECTION CENTER CLOSURE



SINGLE LANE APPROACH INTERSECTION ONE CORNER CLOSURE

* THIS DELINEATION CAN BE REMOVED TO INCREASE CAPACITY OF THE INTERSECTION. POLICE OFFICER CAN BE REMOVED AS WELL

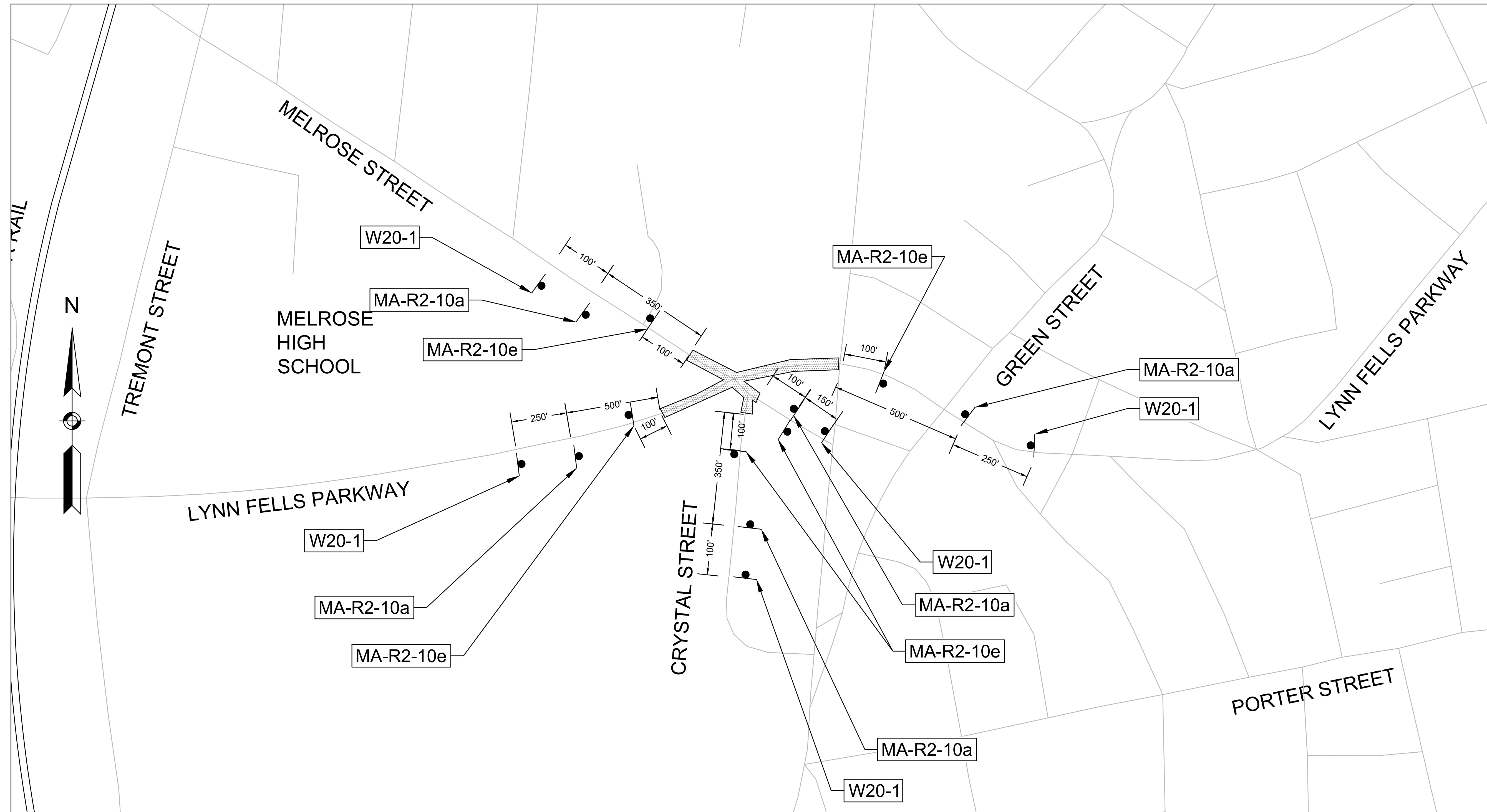


TWO LANE ROAD SHOULDER AND LANE CLOSURE

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INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

SURVEY BY: NTSCH	TEMPORARY TRAFFIC CONTROL PLAN	SHEET NO.
DRAWN BY: KC		25
CHECKED BY: BG	CONT: P20-3361-D1A	SCALE: NTS
APPROVED BY: JM		DATE: 4/11/2023
		25 OF 33



ADVANCE WARNING SIGN PLAN
NTS

NOTES:

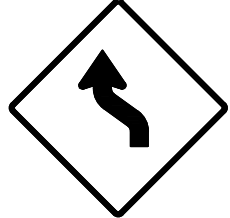
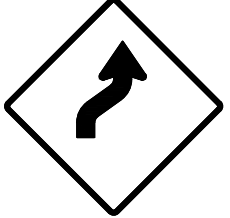









- PCMS SHALL BE INSTALLED AT LEAST 2 WEEKS PRIOR TO THE START OF WORK. WITH THE FOLLOWING WORD MESSAGE:


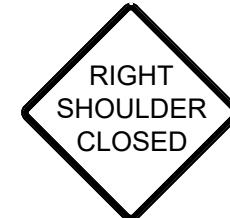
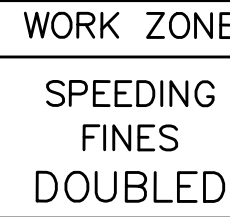
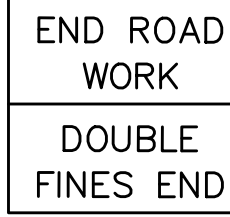

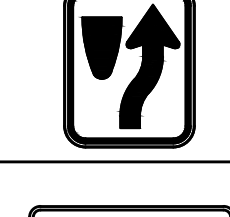
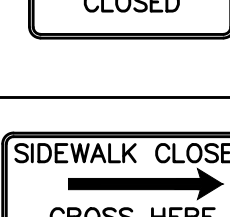
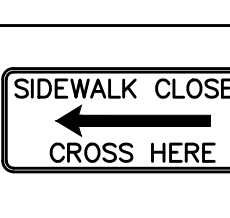

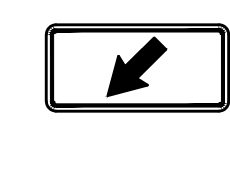

PHASE 1:	PHASE 2:
CONSTR	SEEK
BEGINS	ALT
MM-DD	ROUTE
- THE CONTRACTOR SHALL CONFIRM THE EXACT LOCATION OF THE PCMS WITH THE RESIDENT ENGINEER AND THE CITY OF MELROSE.
- THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER TO CONTINUALLY UPDATE THE PCMS BASED UPON ON-GOING WORK. ABBREVIATIONS SHALL BE IN ACCORDANCE WITH THE MUTCD.
- PLACEMENT OF ADVANCE WARNING SIGN SHALL BE ADJUSTED BASED ON THE ACTUAL FIELD CONDITIONS.
- MA-R2-10e, "END ROAD WORK DOUBLE FINES END" SIGN SHALL BE PLACED AT A MINIMUM OF 100' DOWNSTREAM OF WORK ZONE.

KEY:	
	LIMIT OF WORK
	SIGN
	PORTABLE CHANGEABLE MESSAGE SIGN (PCMS)
	WORK ZONE

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INTERSECTION IMPROVEMENTS AT		
LYNN FELLS PARKWAY AND MELROSE STREET		
MELROSE		
SURVEY BY: NITSCH	TEMPORARY TRAFFIC CONTROL PLAN	SHEET NO.
DRAWN BY: KC		26
CHECKED BY: BG	CONT. P20-3361-D1A	SCALE: NTS
APPROVED BY: JM		DATE: 4/11/2023
		26 OF 33

CONSTRUCTION SIGN SUMMARY

IDENTIFICATION NUMBER	SIZE OF SIGN		SIGN DIAGRAM	COLOR			# REQ'D *	UNIT AREA S.F.	AREA IN SQUARE FEET
	WIDTH	HEIGHT		BACK-GROUND	LEGEND	BORDER			
W1-4L	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W1-4R	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
MA-W4-7L	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	1	9.00	9.00
W5-1	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	4	9.00	36.00
W8-1	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W8-3	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W8-15	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W8-24	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
W13-1p	24"	24"		FLUORESCENT ORANGE	BLACK	BLACK	2	4.00	8.00
W20-1	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	5	9.00	108
W20-4	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	4	9.00	36.00

IDENTIFICATION NUMBER	SIZE OF SIGN		SIGN DIAGRAM	COLOR			# REQ'D *	UNIT AREA S.F.	AREA IN SQUARE FEET
	WIDTH	HEIGHT		BACK-GROUND	LEGEND	BORDER			
MA-W20-7b	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	4	9.00	36.00
W21-5a	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	2	9.00	18.00
MA-R2-10a	48"	36"		FLUORESCENT ORANGE WHITE	BLACK BLACK	BLACK BLACK	5	12.00	144.00
MA-R2-10e	36"	48"		FLUORESCENT ORANGE WHITE	BLACK BLACK	BLACK BLACK	5	12.00	144.00
R3-7R	36"	36"		WHITE	BLACK	BLACK	1	9.00	9.00
R4-7	24"	30"		WHITE	BLACK	BLACK	2	5.00	10.00
R9-9	24"	12"		WHITE	BLACK	BLACK	4	2.00	8.00
R9-11aR	24"	12"		WHITE	BLACK	BLACK	2	2.00	4.00
R9-11aL	24"	12"		WHITE	BLACK	BLACK	2	2.00	4.00
W11-2	36"	36"		FLUORESCENT ORANGE	BLACK	BLACK	4	9.00	36.00
W16-7p	24"	12"		FLUORESCENT ORANGE	BLACK	BLACK	4	2.00	8.00

* NO. OF SIGNS ARE ESTIMATED FOR BIDDING PURPOSES ONLY

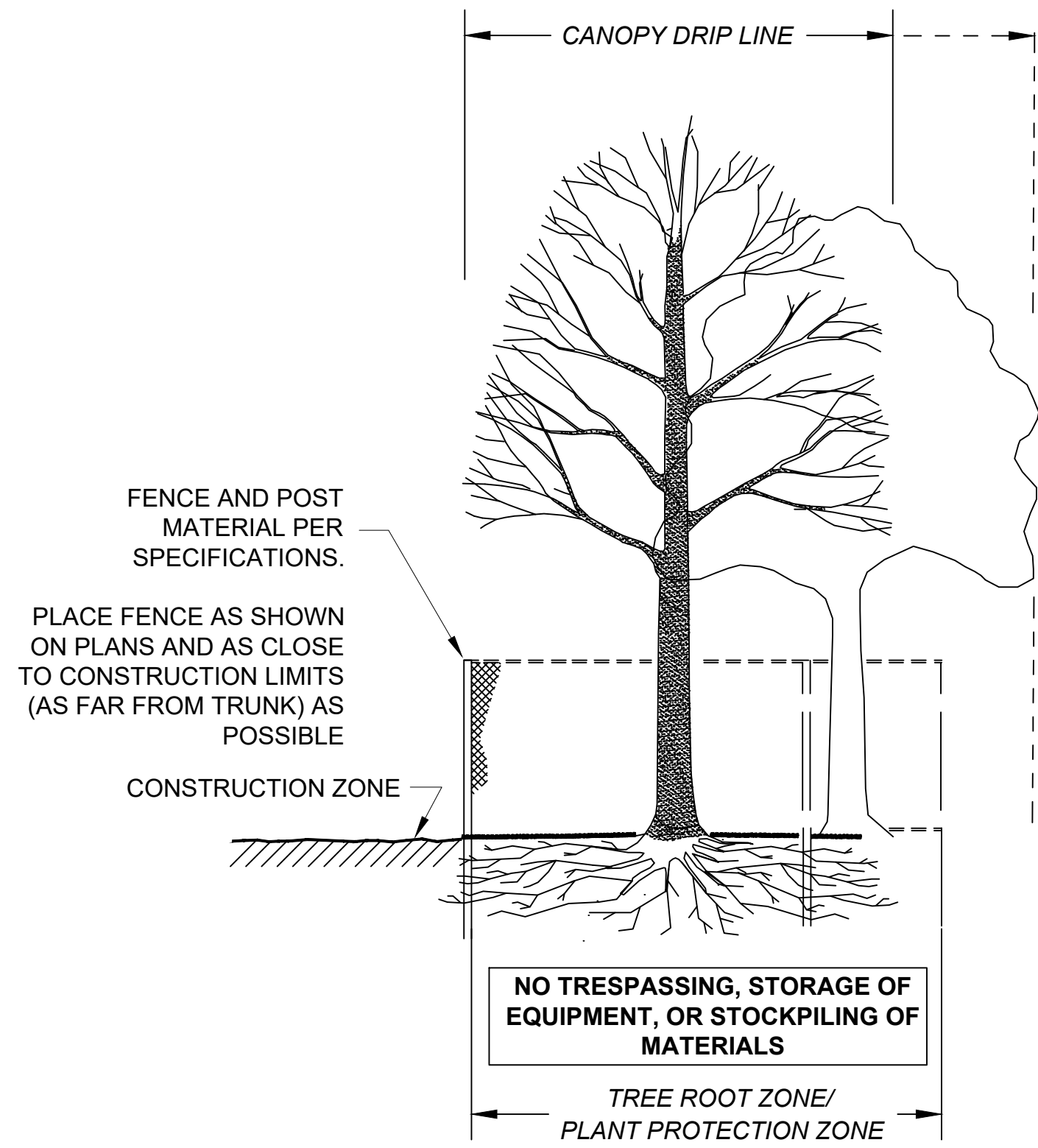
COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION

INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

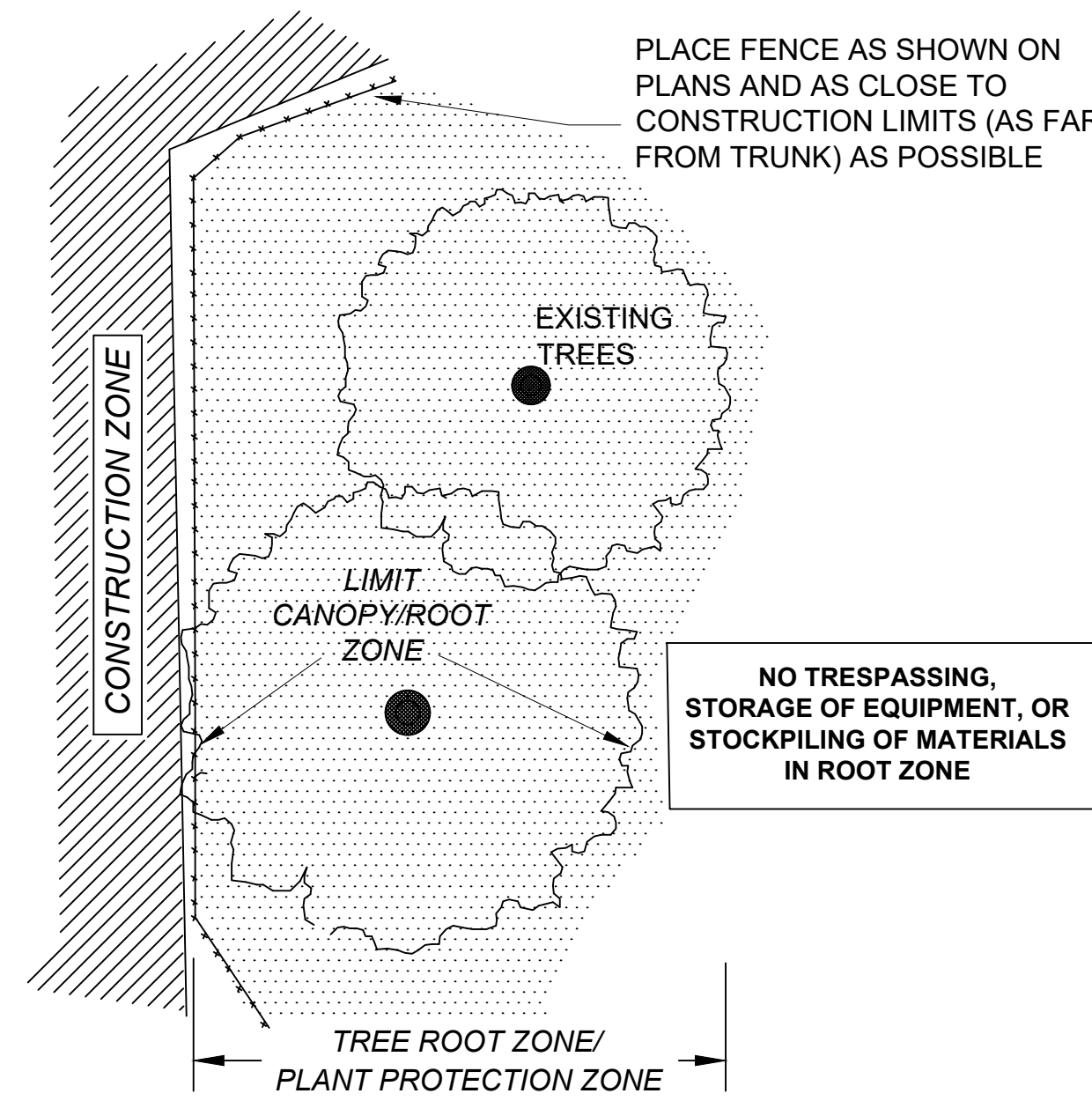
SURVEY BY: NTSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	<p>TEMPORARY TRAFFIC CONTROL PLAN</p> <p>CONT. P20-3361-D1A</p> <p>SCALE: NTS DATE: 4/11/2023</p>	SHEET NO. <p style="text-align: center; font-size: large;">27</p> 27 OF 33
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Nitsch - P:\10000-14999\12589.9 LynnFellsMelrose\Transportation\CAD\12589.9 HD(CONST DETAILS).dwg Aug 3, 2023 8:57 AM

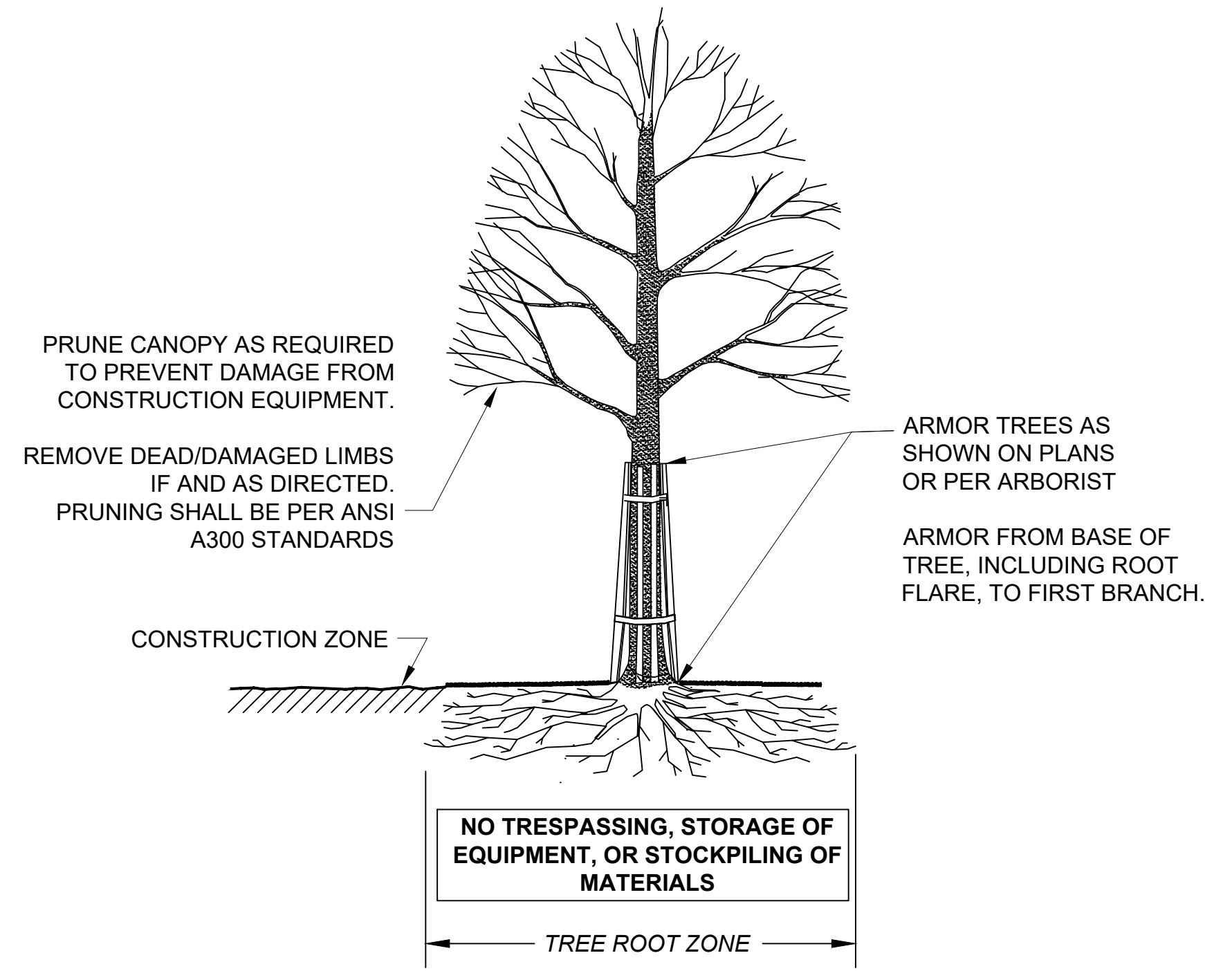
12589.9_HD(CONST DETAILS).DWG Plotted on 3-Aug-2023 8:57 AM



SECTION - FENCE PROTECTION OF ROOT ZONE



PLAN VIEW - FENCE PROTECTION OF ROOT ZONE

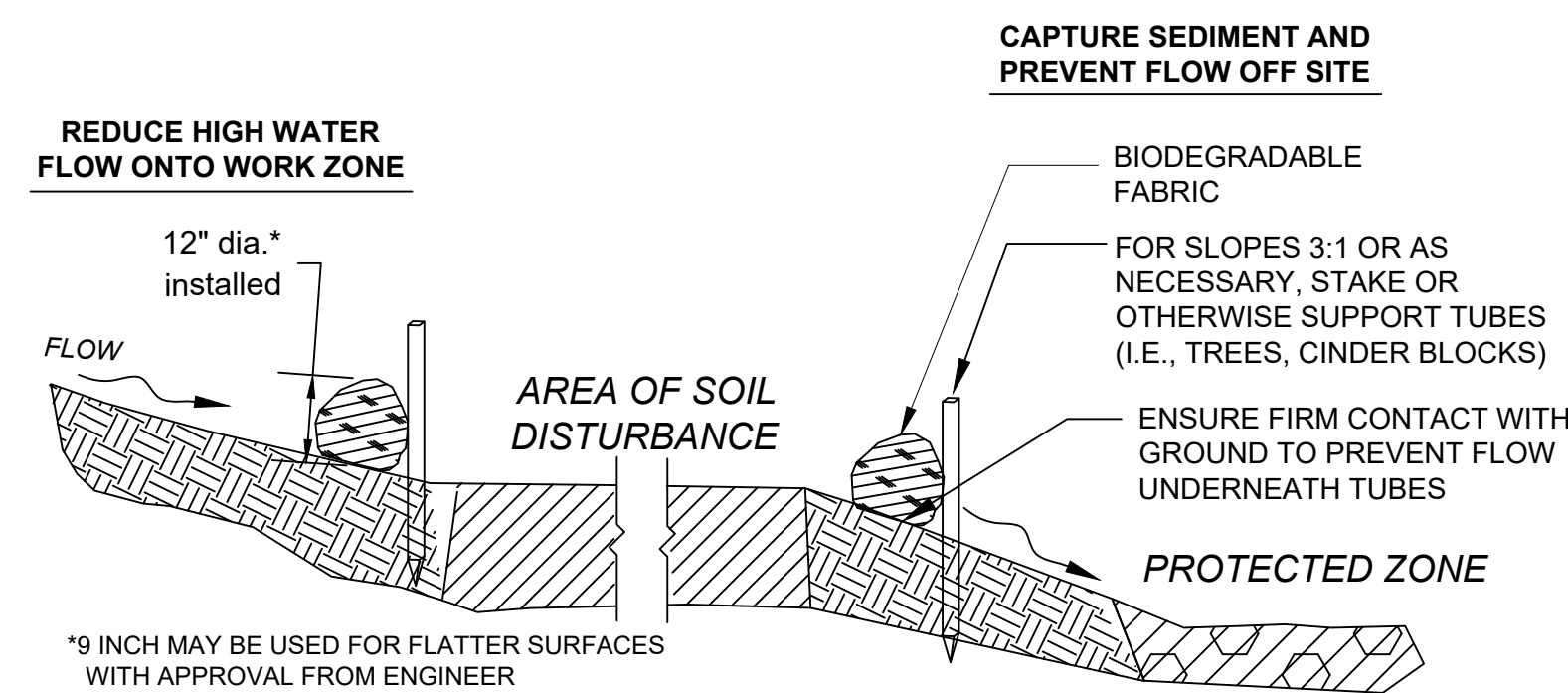


SECTION - TRUNK ARMORING & PRUNING

TREE PROTECTION - ROOT ZONE

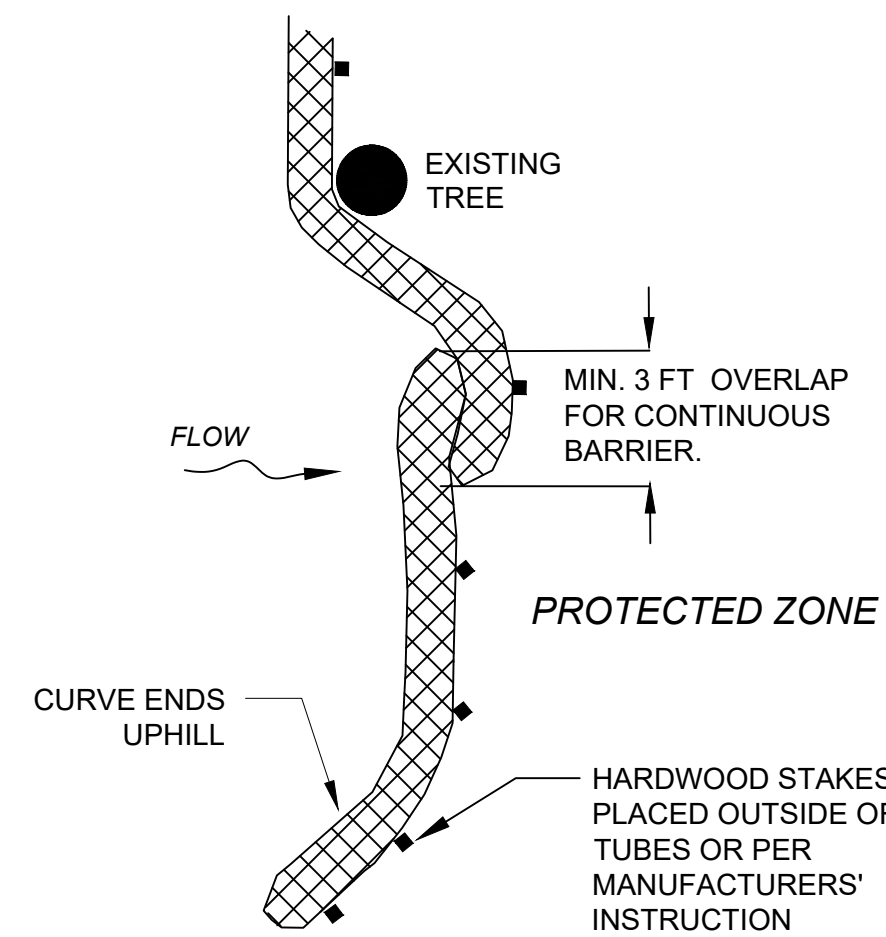
NOT TO SCALE

TREE PROTECTION - TRUNK



SEDIMENT BARRIER - COMPOST FILTER TUBES

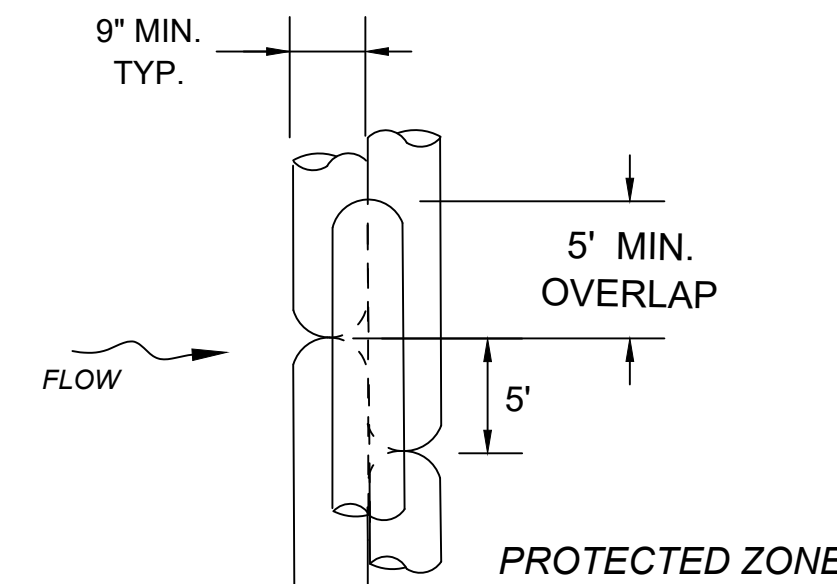
NOT TO SCALE



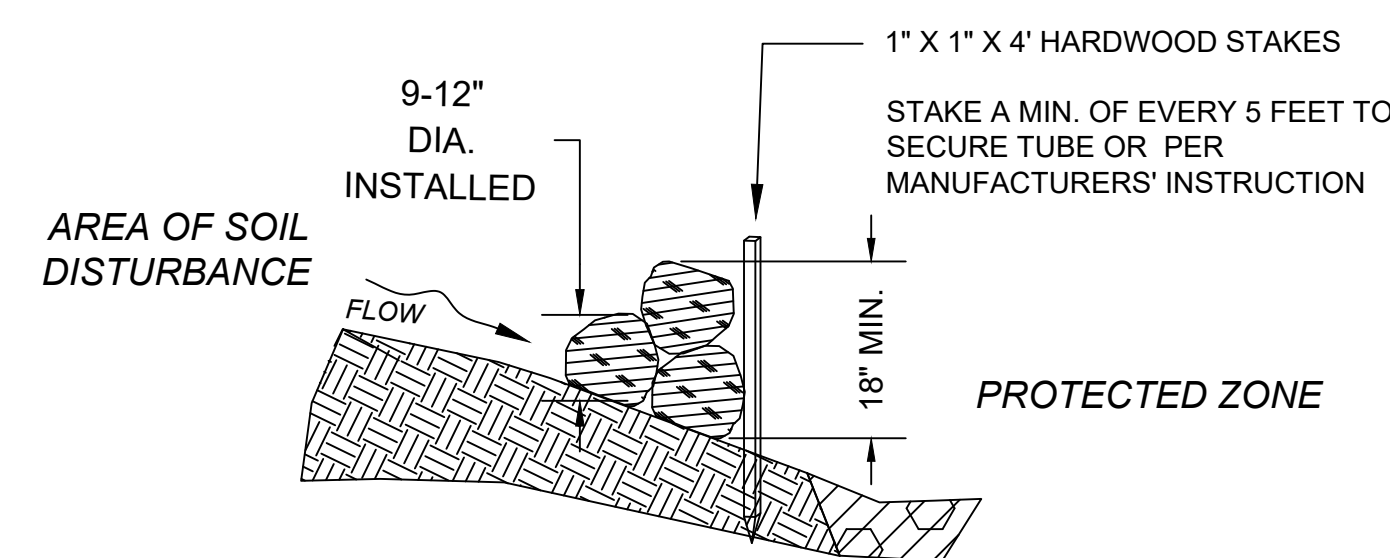
PLACE TUBE ALONG CONTOURS AND PERPENDICULAR TO FLOW.
PLACE AS CLOSE TO LIMIT OF SOIL DISTURBANCE AS POSSIBLE.
ADJUST LOCATION AS REQUIRED FOR OPTIMUM EFFECTIVENESS. DO NOT INSTALL IN WATERWAYS.
PLACE STAKES AS NEEDED TO SECURE TUBES IN PLACE.

COMPOST FILTER TUBE

NOT TO SCALE



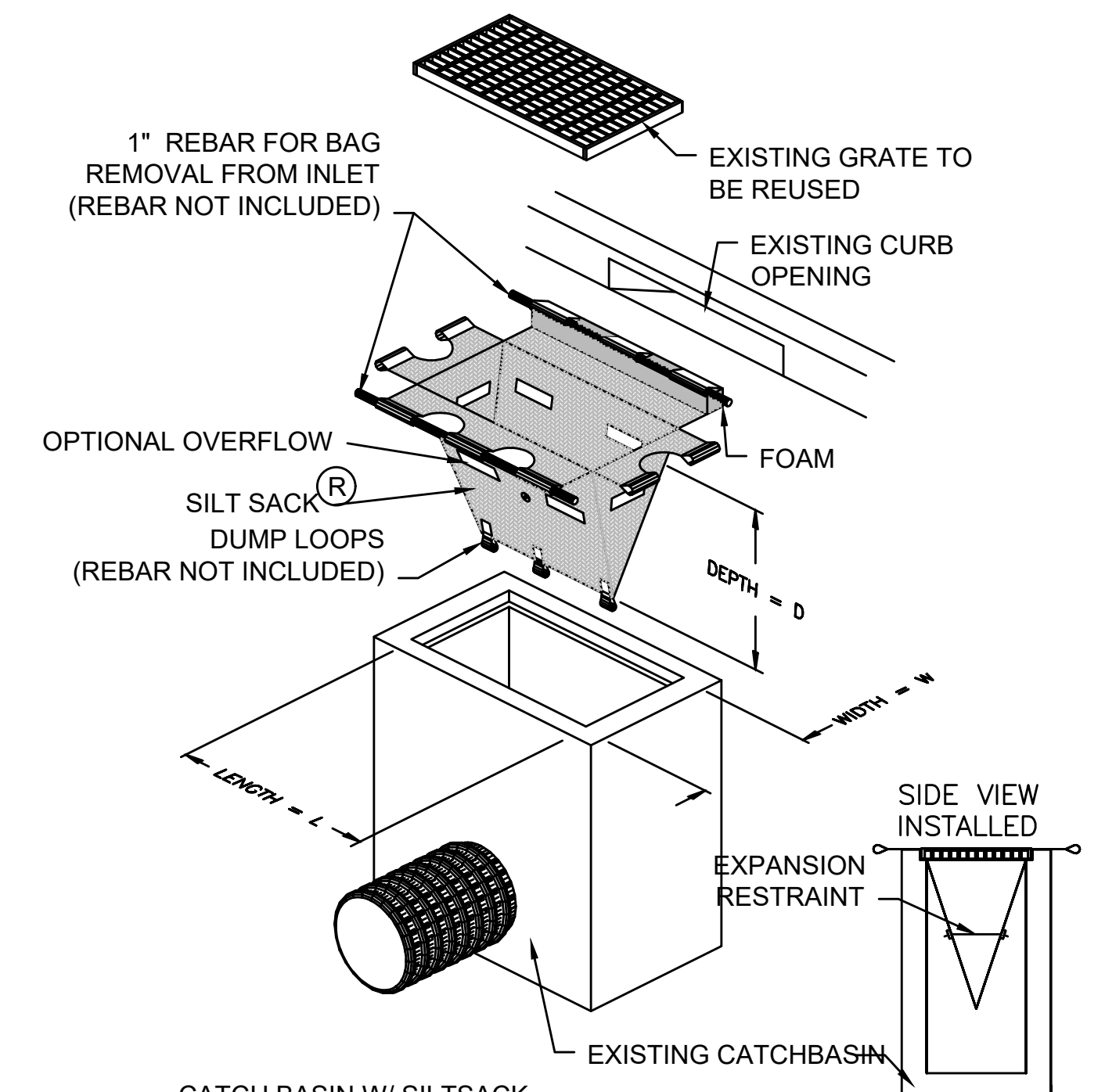
PLAN VIEW



SECTION

COMPOST FILTER TUBE BERM (SLOPES 2:1 OR STEEPER)

NOT TO SCALE

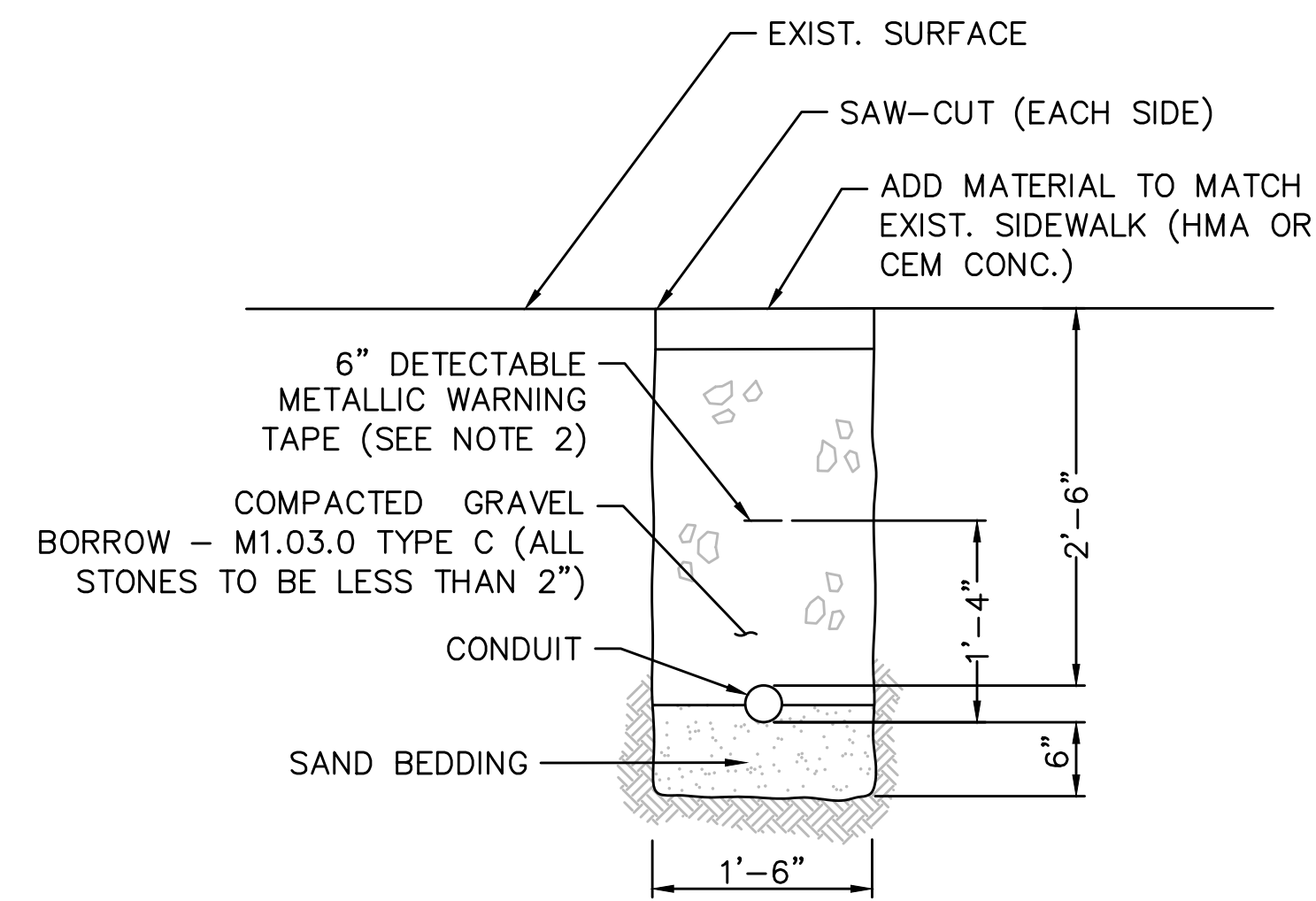


CATCH BASIN W/ SILTSACK
INLET PROTECTION
NOT TO SCALE

COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION

INTERSECTION IMPROVEMENTS AT
LYNN FELLS PARKWAY AND MELROSE STREET
MELROSE

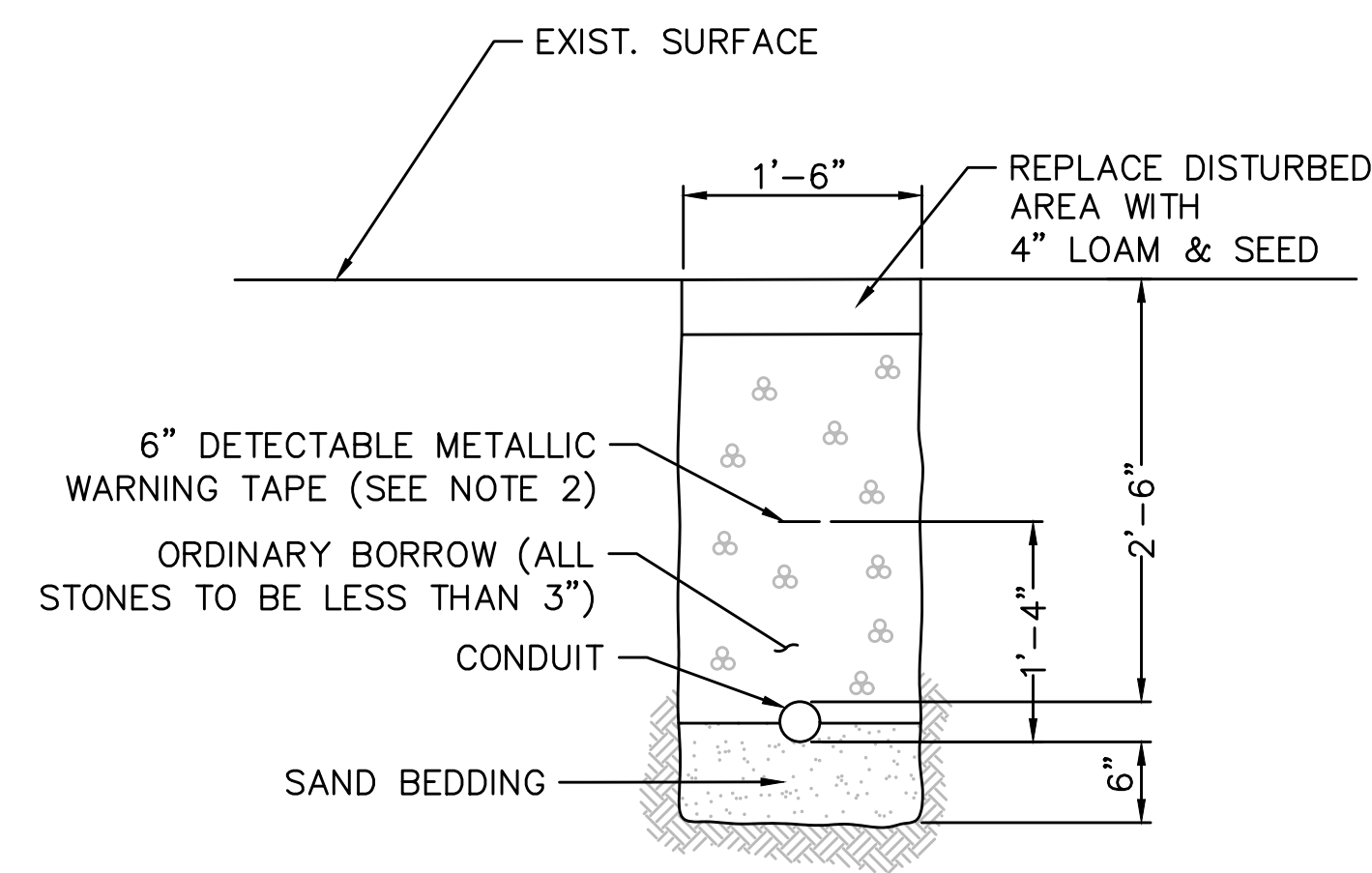
SURVEY BY: NTSCH	CONSTRUCTION DETAILS	SHEET NO.	
DRAWN BY: KC		28	
CHECKED BY: BG		CONT: P20-3361-D1A	SCALE: AS NOTED
APPROVED BY: JM		DATE: 4/11/2023	28 OF 33



NOTE:

1. SCHEDULE 80 ELECTRICAL CONDUIT TYPE NM-PLASTIC (UL), WITH PULL ROPE, UNLESS OTHERWISE APPROVED BY MASSDOT.
2. WARNING TAPE SHALL BE PER CURRENT APWA STANDARDS.

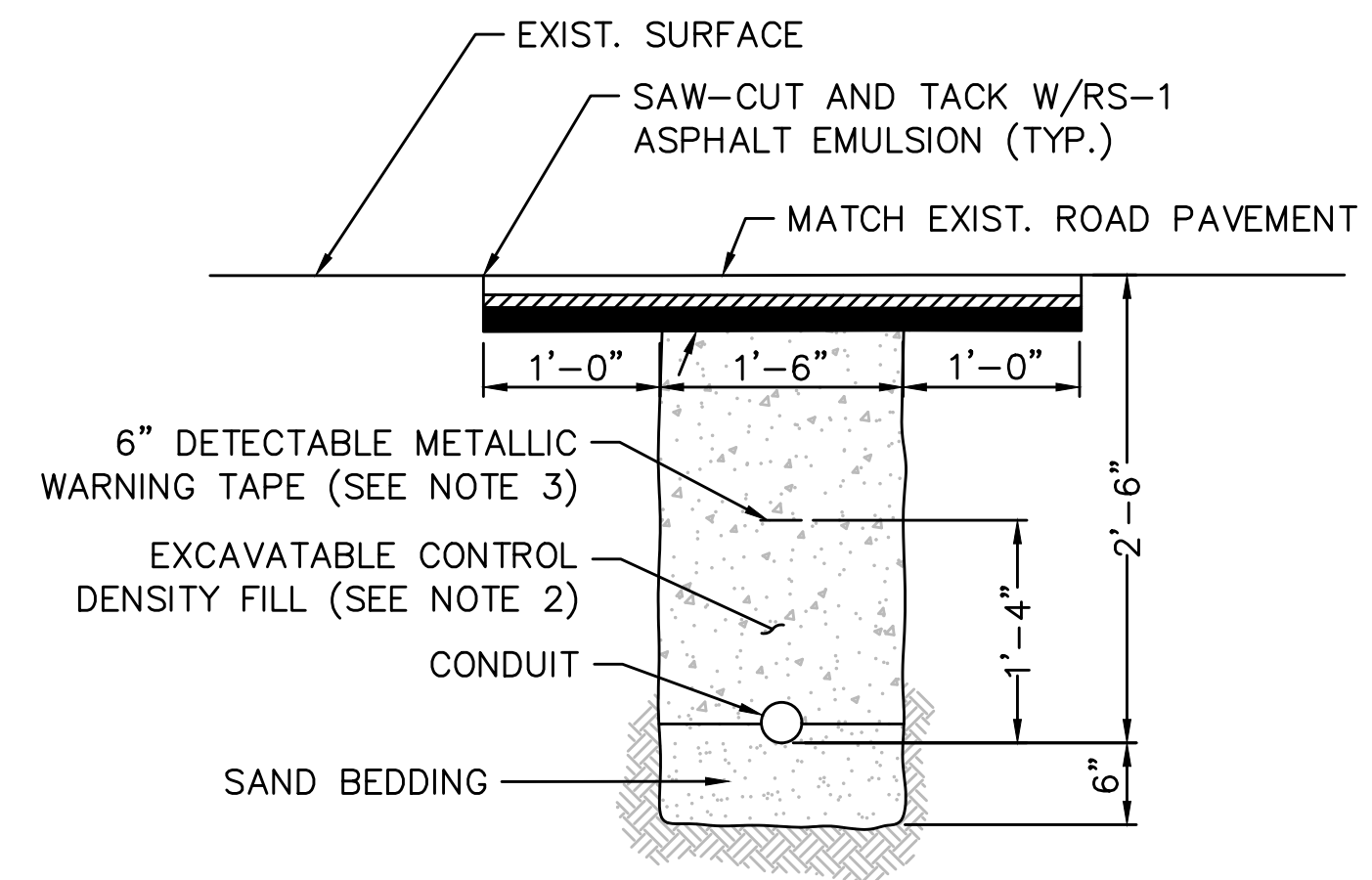
CONDUIT UNDER SIDEWALK
NOT TO SCALE



NOTE:

1. SCHEDULE 80 ELECTRICAL CONDUIT TYPE NM-PLASTIC (UL), WITH PULL ROPE, UNLESS OTHERWISE APPROVED BY MASSDOT.
2. WARNING TAPE SHALL BE PER CURRENT APWA STANDARDS.

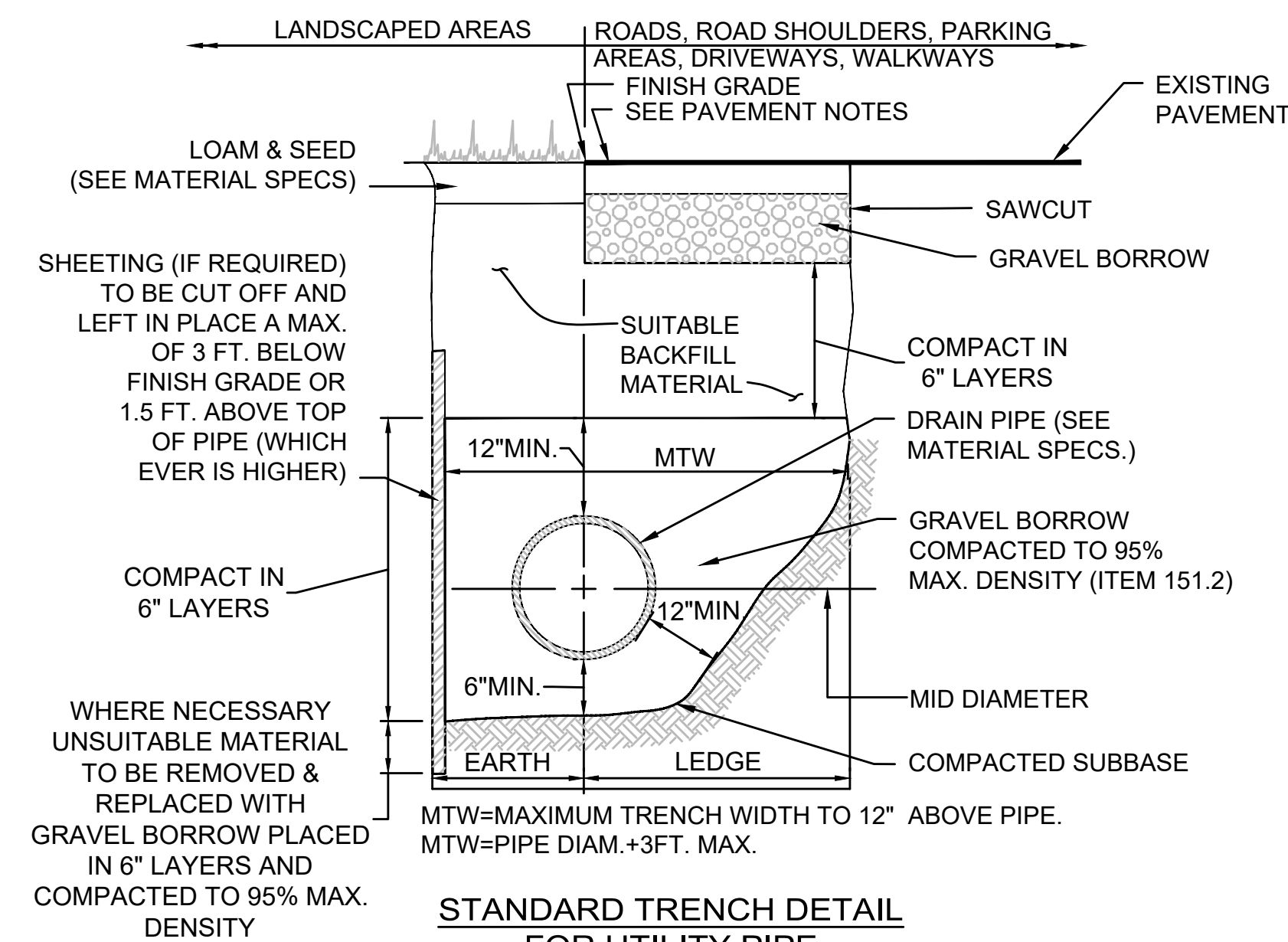
CONDUIT UNDER GRASS AREAS
NOT TO SCALE



NOTE:

1. SCHEDULE 80 ELECTRICAL CONDUIT TYPE NM-PLASTIC (UL), WITH PULL ROPE, UNLESS OTHERWISE APPROVED BY MASSDOT.
2. CONTROL DENSITY FILL SHALL MEET THE REQUIREMENTS OF MASSDOT SUBSECTION M4.08.0, TYPE 2E.
3. WARNING TAPE SHALL BE PER CURRENT APWA STANDARDS.

CONDUIT CROSSING ROADWAY
NOT TO SCALE

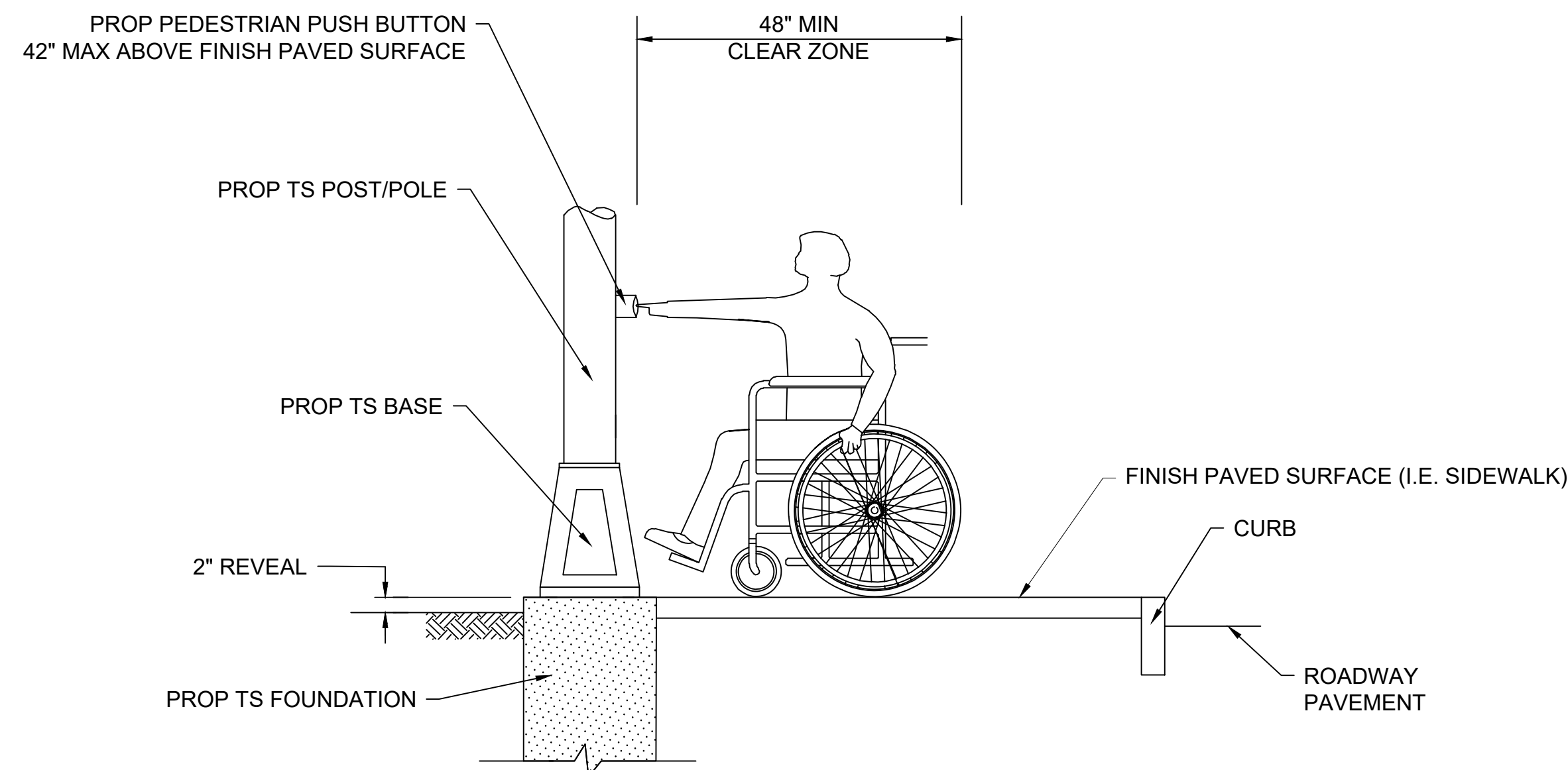


STANDARD TRENCH DETAIL
FOR UTILITY PIPE
NOT TO SCALE

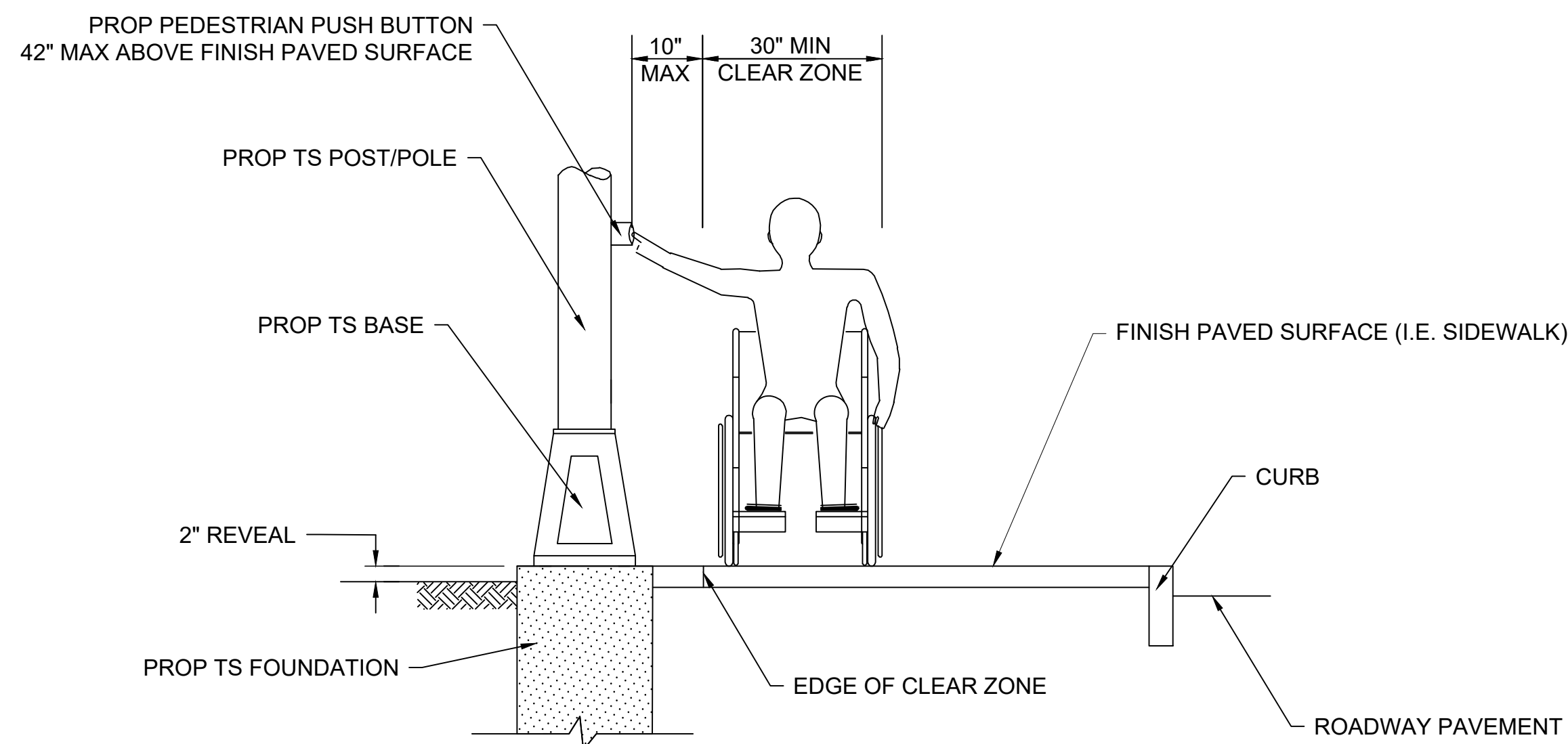
NOTES:

1. FOR PERMANENT PATCH IN ROADWAY AREAS WITH MILL AND OVERLAY, USE FULL DEPTH PAVEMENT MATERIAL SHOWN ON TYPICAL SECTIONS (PAID FOR UNDER ITEM 451.)
2. FOR TEMPORARY PATCH IN ROADWAY AREAS WITH FULL DEPTH PAVEMENT USE 3.5 INCHES HMA BINDER MIX. (PAID FOR UNDER ITEM 472.)

COMMONWEALTH OF MASSACHUSETTS			
DEPARTMENT OF CONSERVATION AND RECREATION			
DIVISION OF ENGINEERING AND			
DIVISION OF PLANNING AND RESOURCE PROTECTION			
INTERSECTION IMPROVEMENTS AT			
LYNN FELLS PARKWAY AND MELROSE STREET			
MELROSE			
SURVEY BY: NITSCH	CONSTRUCTION DETAILS		SHEET NO.
DRAWN BY: KC			29
CHECKED BY: BG	CONT. P20-3361-D1A	SCALE: AS NOTED	
APPROVED BY: JM		DATE: 4/11/2023	29 OF 33



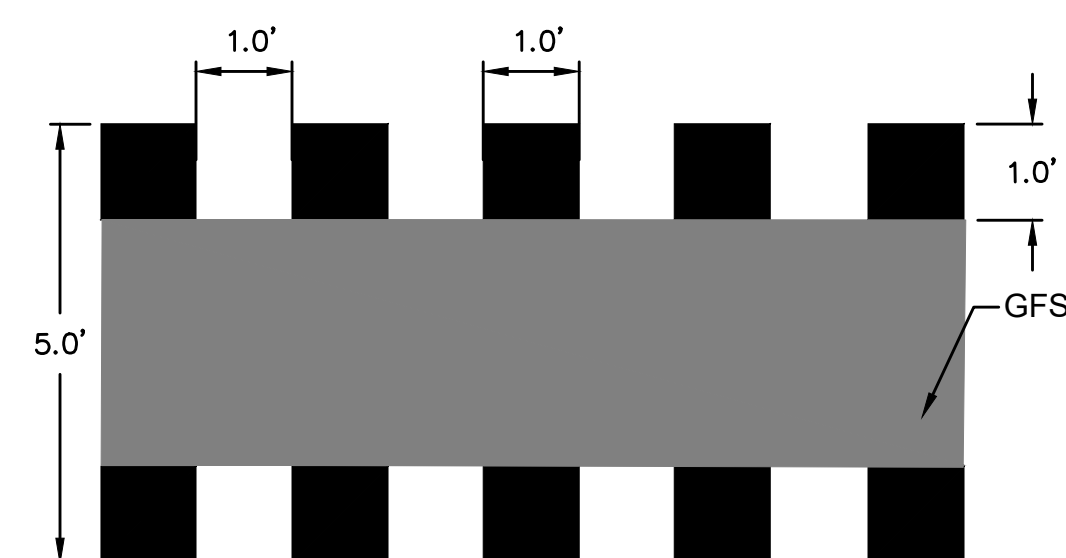
FORWARD APPROACH (SIDE REACH)
NOT TO SCALE



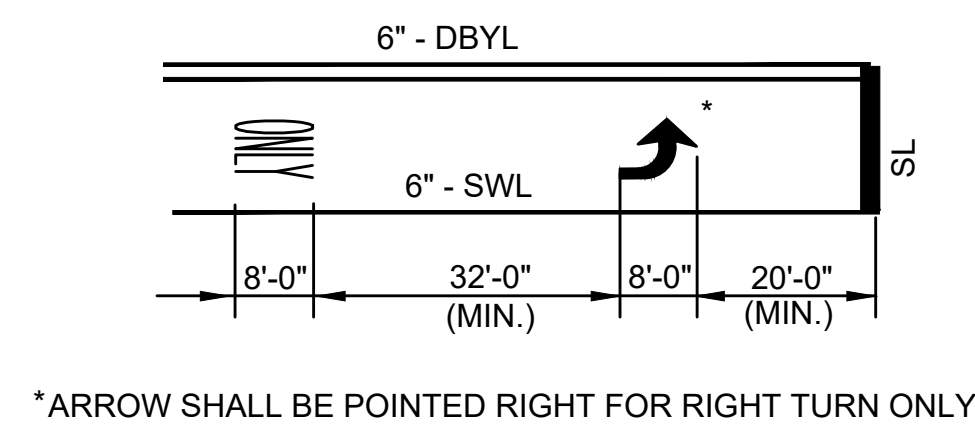
PARALLELL APPROACH (SIDE REACH)

- NOTES:
- A CLEAR GROUND SPACE SHALL CONSIST OF A STABLE AND FIRM AREA, COMPLYING WITH 521 WMR 6.5 (FORWARD REACH) OR 521 CMR 6.6 (SIDE REACH) AND SHALL BE PROVIDED AT EACH OF THE PEDESTRIAN PUSH BUTTONS.
 - WHERE A FORWARD APPROACH IS PROVIDED, PEDESTRIAN PUSH BUTTONS SHALL BE WITHIN TEN INCHES (10") HORIZONTALLY OF AND CENTERED ON THE CLEAR GROUND SPACE.
 - WHERE A PARALLEL APPROACH IS PROVIDED, PEDESTRIAN PUSH BUTTONS SHALL BE WITHIN TEN INCHES (10") HORIZONTALLY OF AND CENTERED ON THE CLEAR GROUND SPACE.

PEDESTRIAN PUSH BUTTON CLEAR ZONE
NOT TO SCALE

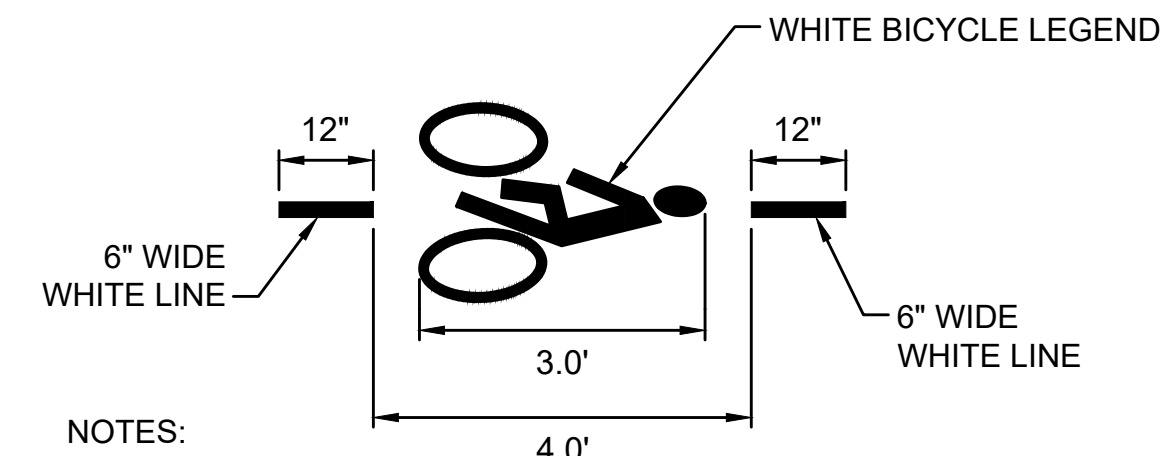


DOTTED WHITE LINE EXTENSION BICYCLE CROSSING FOR SEPARATED BIKE LANE



*ARROW SHALL BE POINTED RIGHT FOR RIGHT TURN ONLY.

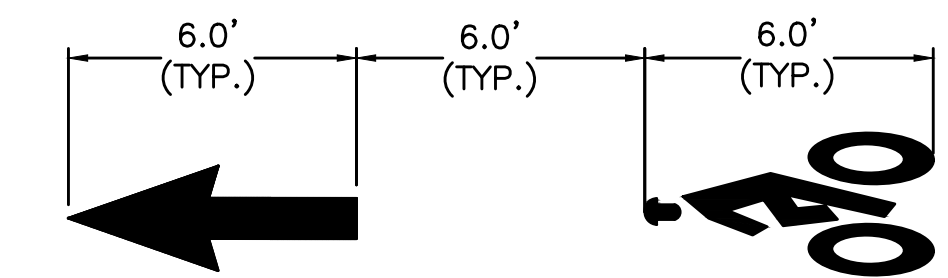
NOT TO SCALE



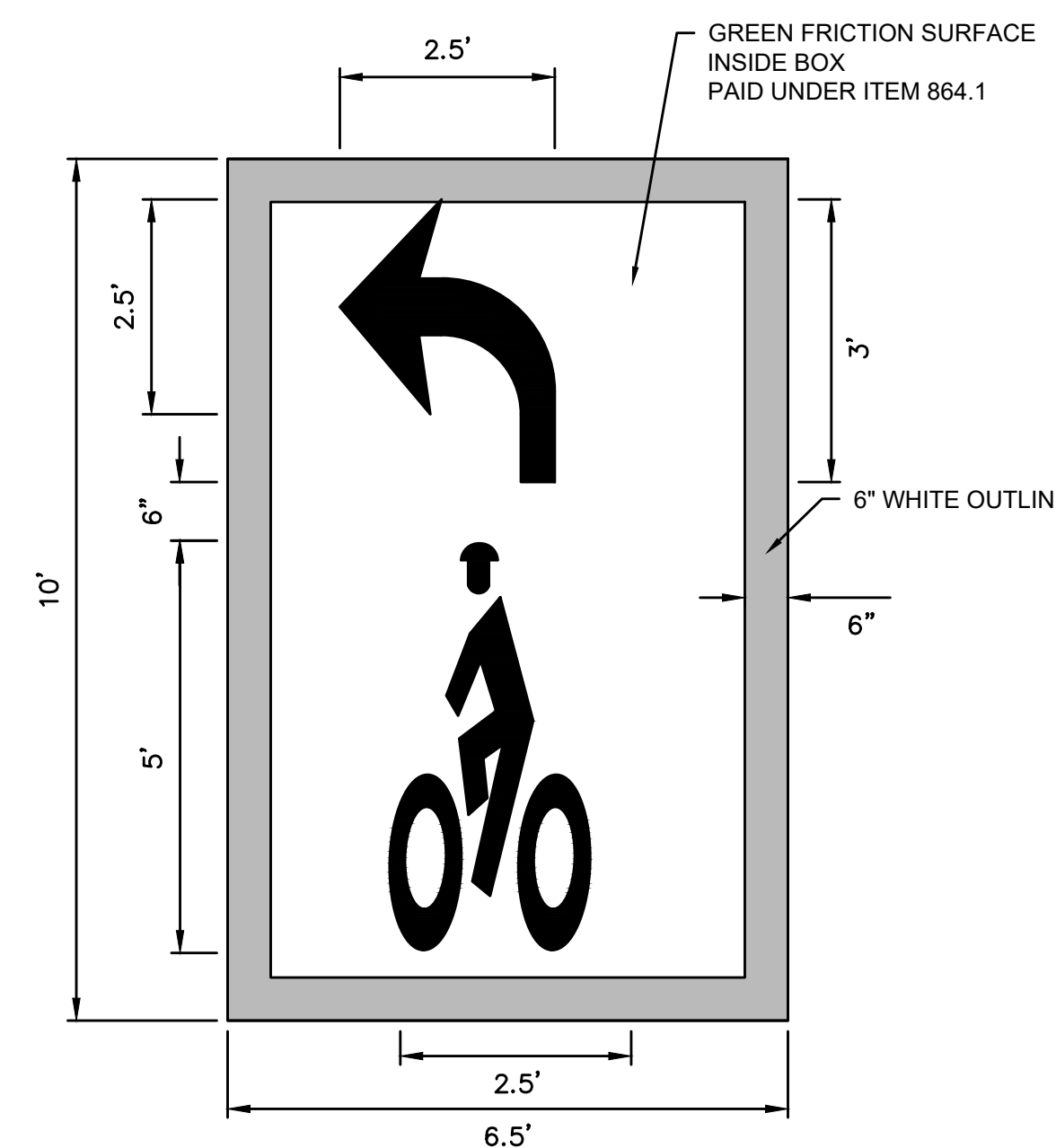
NOTES:

- ALL PROPOSED BICYCLE DETECTOR PAVEMENT MARKINGS SHALL BE REFLECTORIZED (TAPE) THERMOPLASTIC.

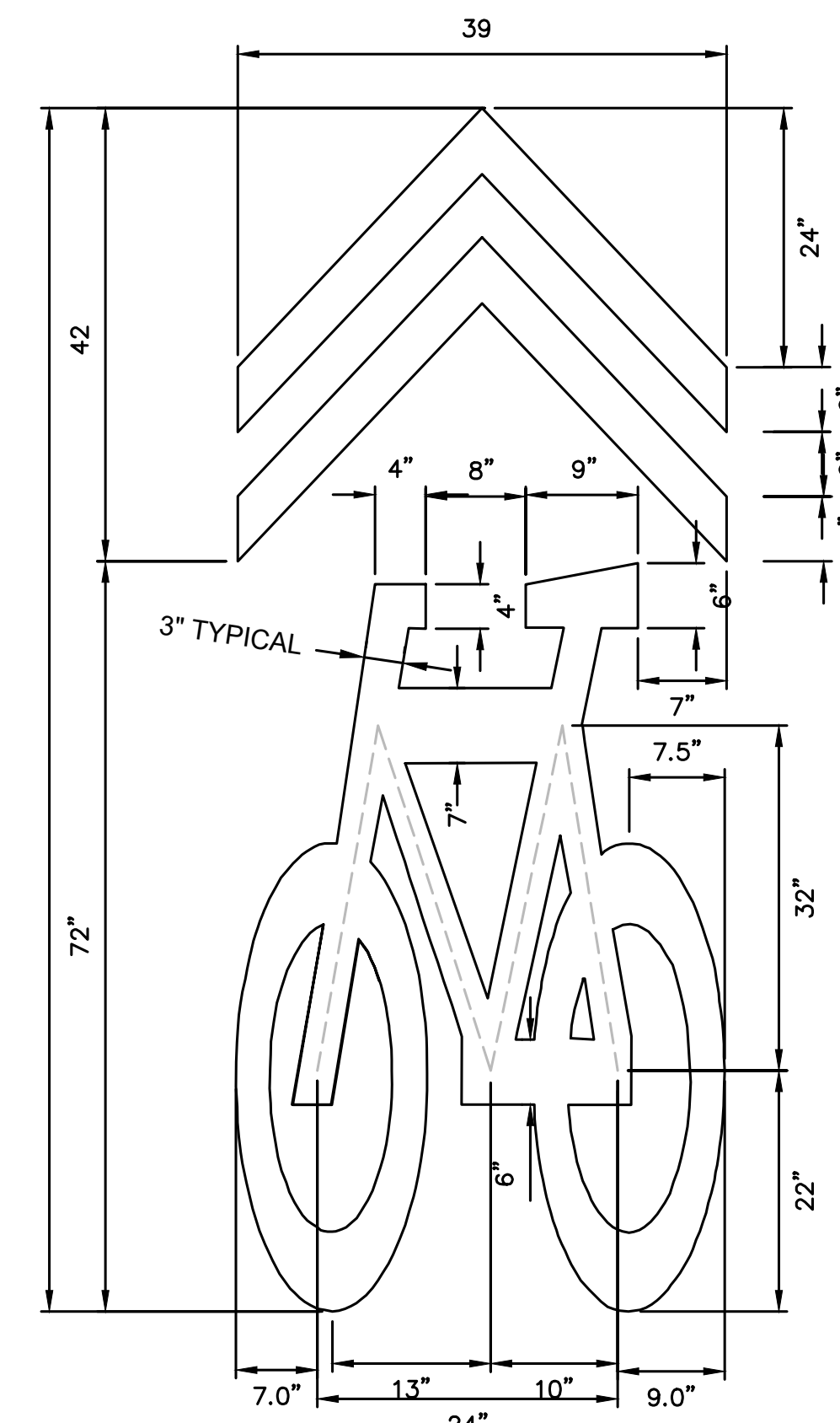
BICYCLE DETECTOR PAVEMENT MARKING
NOT TO SCALE



BICYCLE WITH ARROW PAVEMENT MARKING
NOT TO SCALE



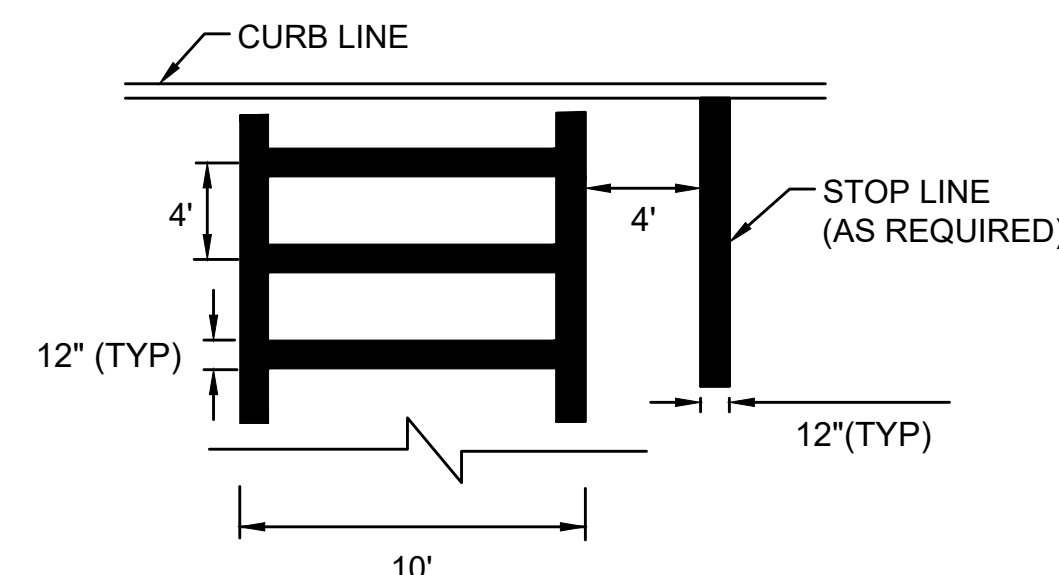
BICYCLE TWO STAGE TURN BOX
NOT TO SCALE



SHARED LANE SYMBOL PLACEMENT NOTES:

- SHARED LANE MARKING SYMBOLS SHALL BE PLACED SO THAT THE CENTER IS 4 FEET FROM THE ADJACENT SOLID WHITE EDGE LINE.
- DO NOT PLACE SYMBOLS ON LANE LINES.
- SHARED LANE SYMBOLS SHALL BE PLACED A MAXIMUM OF 500 FEET APART.

SHARED LANE MARKING DETAIL
NOT TO SCALE

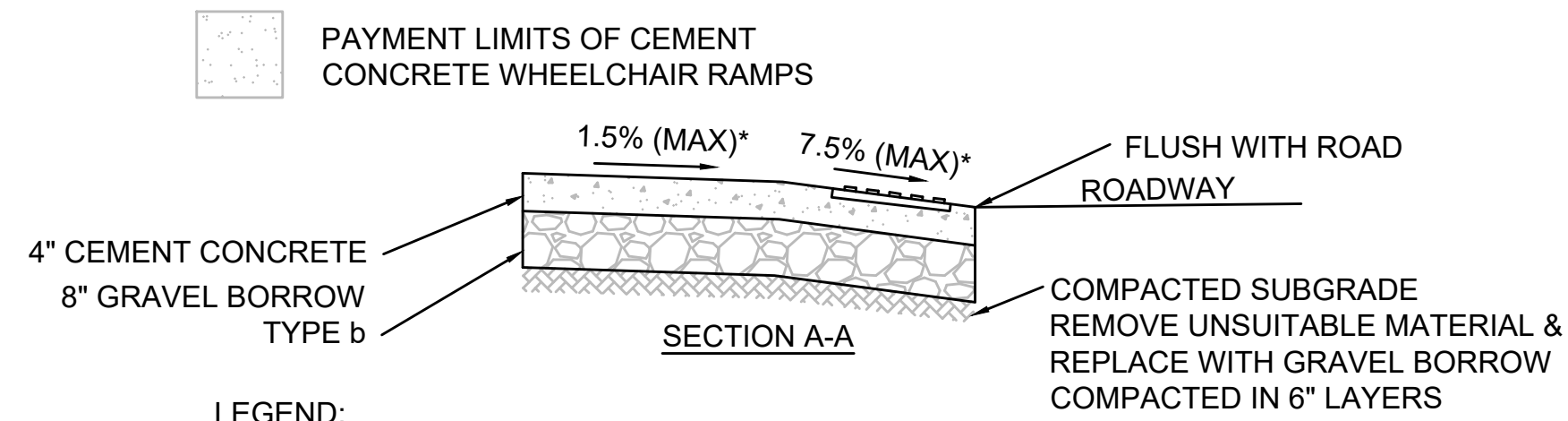
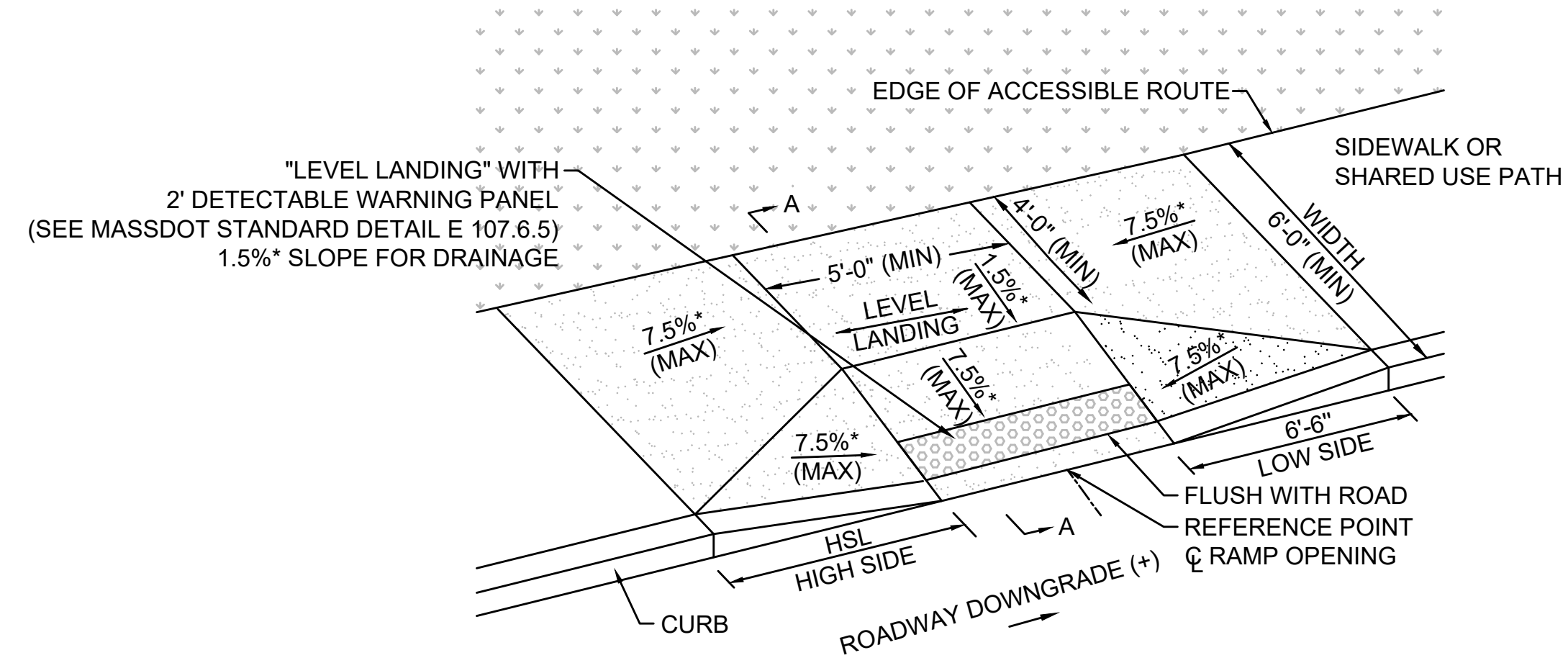


NOTES:

- ALL PROPOSED STOP LINES AND CROSSWALKS SHALL BE WHITE REFLECTORIZED THERMOPLASTIC.

HIGH VISIBILITY CROSSWALK & STOP LINE
NOT TO SCALE

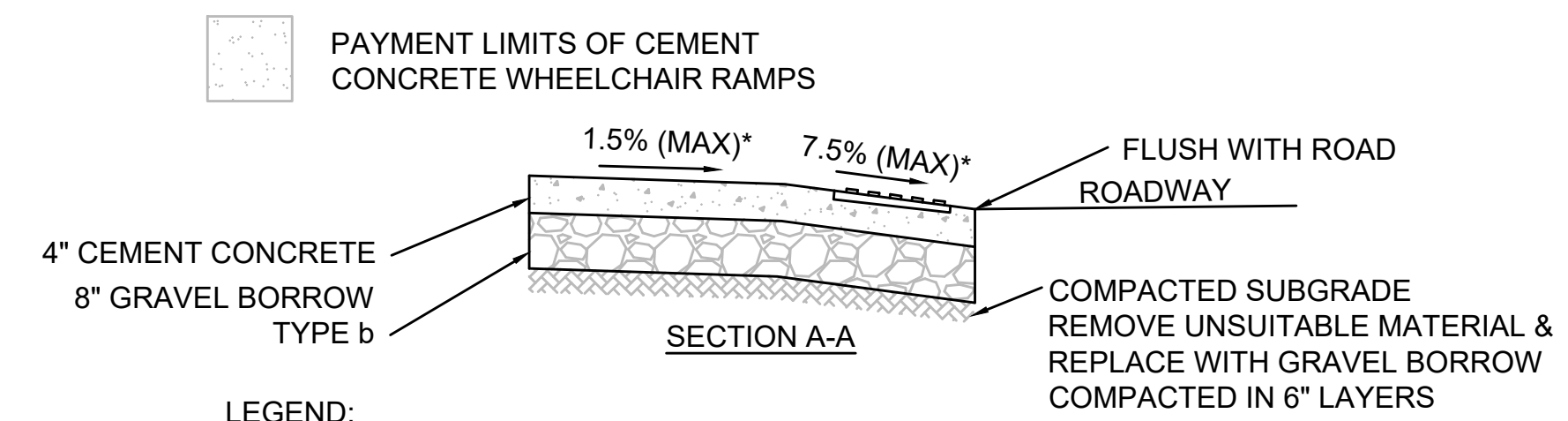
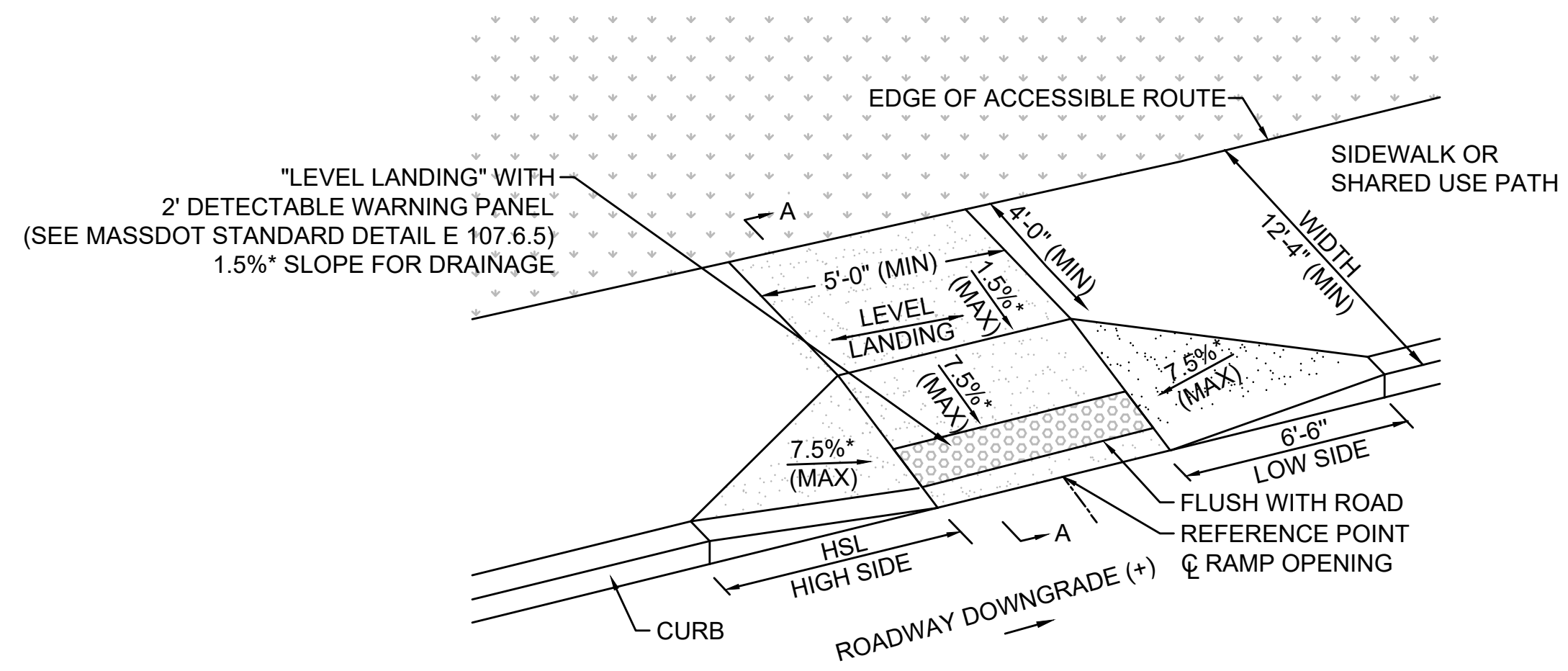
COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF CONSERVATION AND RECREATION DIVISION OF ENGINEERING AND DIVISION OF PLANNING AND RESOURCE PROTECTION			
INTERSECTION IMPROVEMENTS AT LYNN FELS PARKWAY AND MELROSE STREET MELROSE			
CONSTRUCTION DETAILS		SHEET NO. 30	
SURVEY BY: NITSCH DRAWN BY: KC		SCALE: AS NOTED	
CHECKED BY: BG		DATE: 4/11/2023	
APPROVED BY: JM		CONT: P20-3361-D1A	
30 OF 33			



LEGEND:
 * = TOLERANCE FOR CONSTRUCTION ±0.5%
 HSL = HIGH SIDE TRANSITION LENGTH
 (SEE E 107.9.0)

SEE MASSDOT STANDARD DRAWING NUMBER E107.2.0. FOR
 WHEEL CHAIR RAMP LESS THAN 12'-4\"/>

WHEELCHAIR RAMP TYPE C
 NOT TO SCALE



LEGEND:
 * = TOLERANCE FOR CONSTRUCTION ±0.5%
 HSL = HIGH SIDE TRANSITION LENGTH
 (SEE E 107.9.0)

SEE MASSDOT STANDARD DRAWING NUMBER E107.2.0. FOR
 WHEEL CHAIR RAMP GREATER THAN 12'-4\"/>

WHEELCHAIR RAMP TYPE B
 NOT TO SCALE

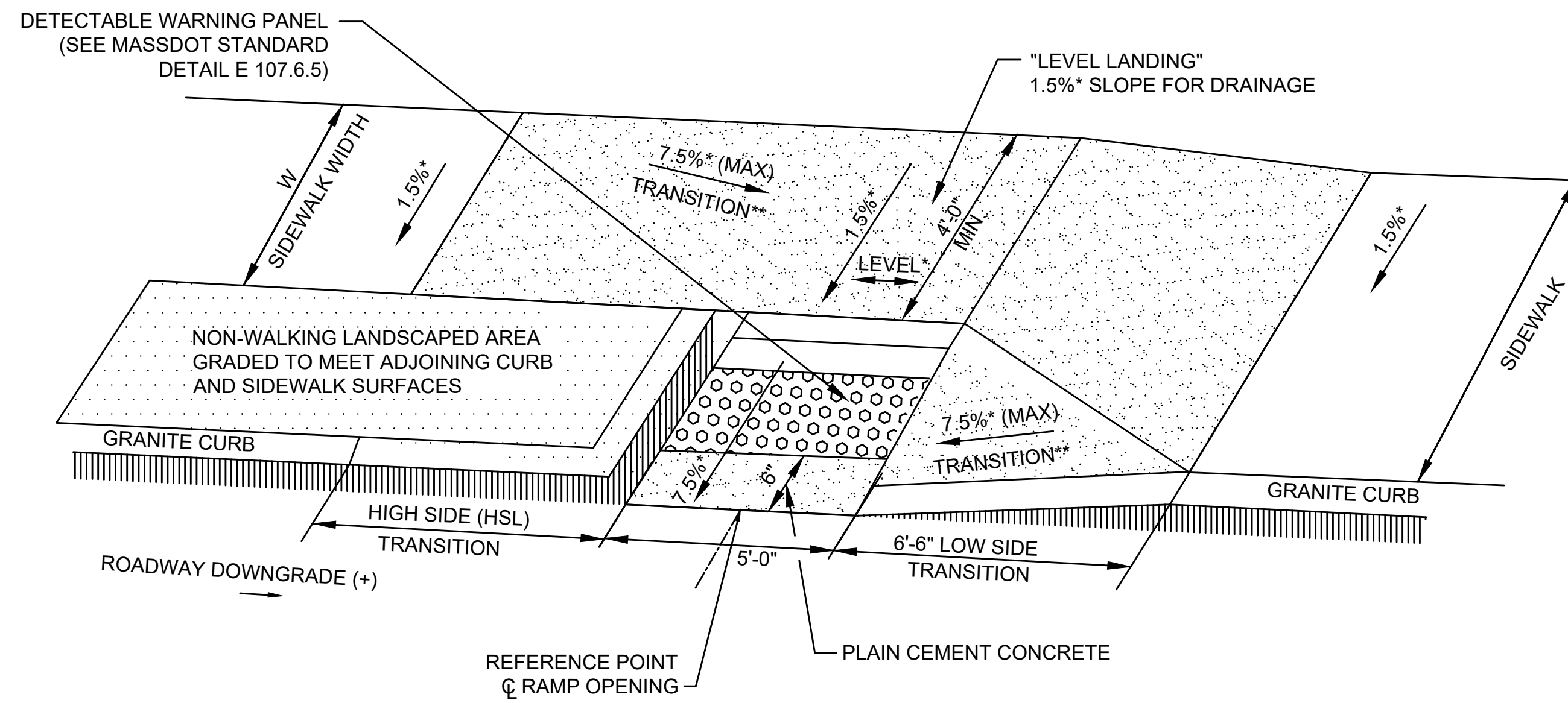
PEDESTRIAN CURB RAMP DATA										
PCR NUMBER	STREET NAME &	STATION	OFFSET	RAMP WIDTH	SIDEWALK WIDTH	GUTTER SLOPE	LEFT SIDE		RIGHT SIDE	
							CURB REVEAL	CURB TRANSITION LENGTH	CURB REVEAL	CURB TRANSITION LENGTH
6	MELROSE STREET	23+08.62	16.00	8'-0"	12'-0"	0.82%	0'-6"	7'-8"	0'-6"	6'-6"
10	LYNN FELLS PARKWAY	7+70.72	22.43	5'-0"	10'-0"	3.17%	0'-6"	6'-6"	0'-6"	6'-6"
11	LYNN FELLS PARKWAY	7+72.26	22.00	5'-0"	10'-0"	3.17%	0'-6"	6'-6"	0'-3"	7'-0"
12	LYNN FELLS PARKWAY	7+86.21	37.67	5'-0"	10'-0"	3.17%	0'-3"	7'-0"	0'-6"	6'-6"

PEDESTRIAN CURB RAMP DATA										
PCR NUMBER	STREET NAME &	STATION	OFFSET	RAMP WIDTH	SIDEWALK WIDTH	GUTTER SLOPE	LEFT SIDE		RIGHT SIDE	
							CURB REVEAL	CURB TRANSITION LENGTH	CURB REVEAL	CURB TRANSITION LENGTH
1	LYNN FELLS PARKWAY	5+26.18	22.00	5'-0"	5'-0"	2.11%	0'-6"	6'-6"	0'-6"	11'-0"
2	MELROSE STREET	22+29.41	29.41	10'-0"	10'-0"	0.56%	0'-6"	6'-6"	0'-6"	7'-8"
5	LYNN FELLS PARKWAY	4+59.25	12.00	5'-0"	VARIABLES	2.05%	0'-6"	11'-0"	0'-6"	6'-6"
7	CRYSTAL STREET	31+20.18	14.24	5'-0"	9'-0"	0.82%	0'-6"	7'-8"	0'-6"	6'-6"

COMMONWEALTH OF MASSACHUSETTS
DEPARTMENT OF CONSERVATION AND RECREATION
DIVISION OF ENGINEERING AND
DIVISION OF PLANNING AND RESOURCE PROTECTION

INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

SURVEY BY: NITSCH	CONSTRUCTION DETAILS	SHEET NO.
DRAWN BY: KC		31
CHECKED BY: BG		
APPROVED BY: JM		
CONT: P20-3361-D1A	SCALE: AS NOTED	DATE: 4/11/2023
		31 OF 33



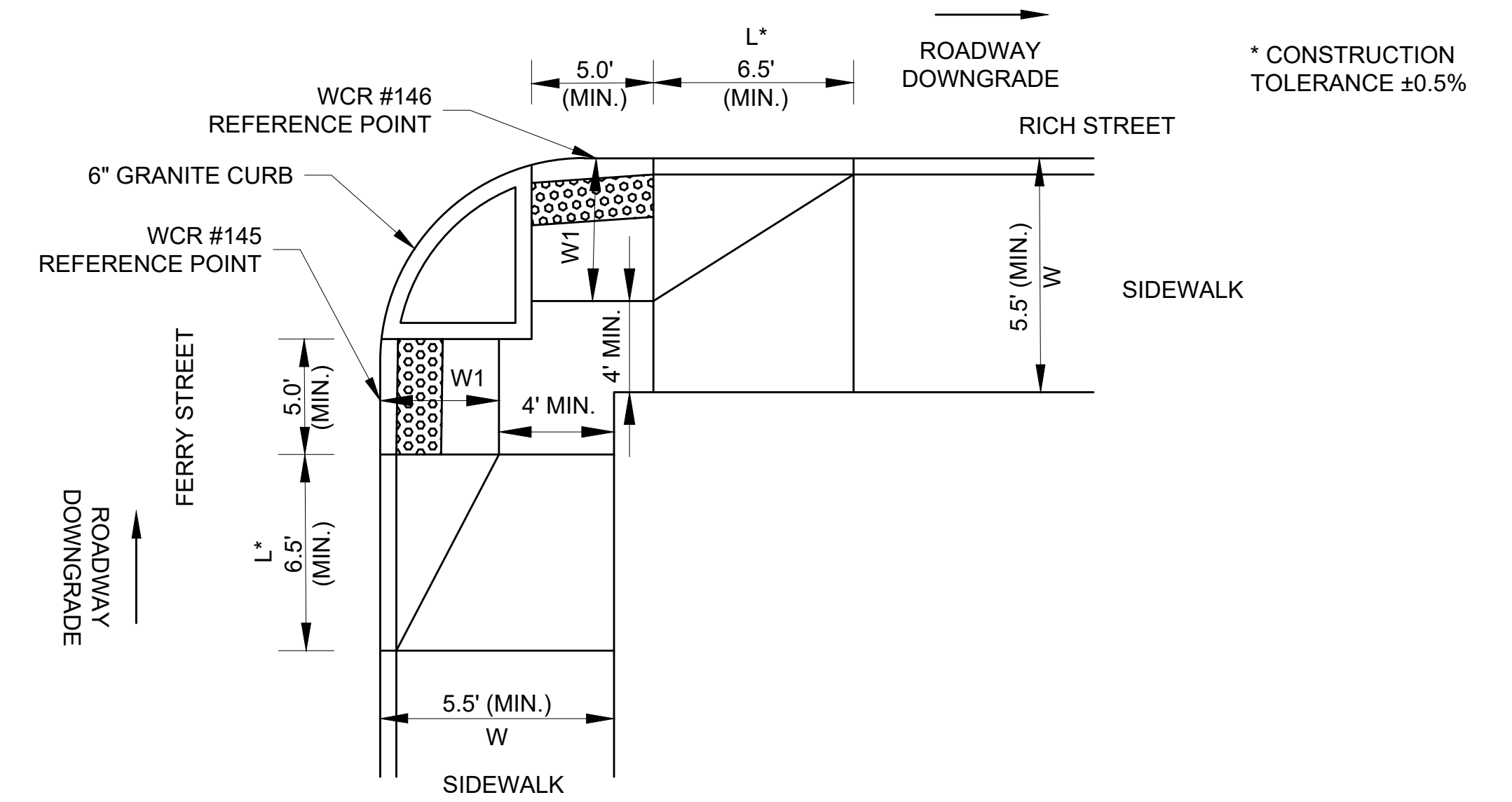
LEGEND:

* = TOLERANCE FOR CONSTRUCTION ±0.5%
 HSL = HIGH SIDE TRANSITION LENGTH
 (SEE E 107.9.0)

PAYMENT LIMITS OF CEMENT CONCRETE WHEELCHAIR RAMPS

PEDESTRIAN CURB RAMP WITH
 LANDSCAPED STRIP - TYPE B
 NOT TO SCALE

PEDESTRIAN CURB RAMP DATA										
PCR NUMBER	STREET NAME &	STATION	OFFSET	RAMP WIDTH	SIDEWALK WIDTH	GUTTER SLOPE	LEFT SIDE		RIGHT SIDE	
							CURB REVEAL	CURB TRANSITION LENGTH	CURB REVEAL	CURB TRANSITION LENGTH
3	LYNN FELLS PARKWAY	4+41.43	33.16	5'-0"	VARIABLES	0.65%	0'-6"	N/A	0'-6"	7'-8"
4	LYNN FELLS PARKWAY	4+34.95	24.22	5'-0"	VARIABLES	0.67%	0'-6"	6'-6"	0'-6"	N/A



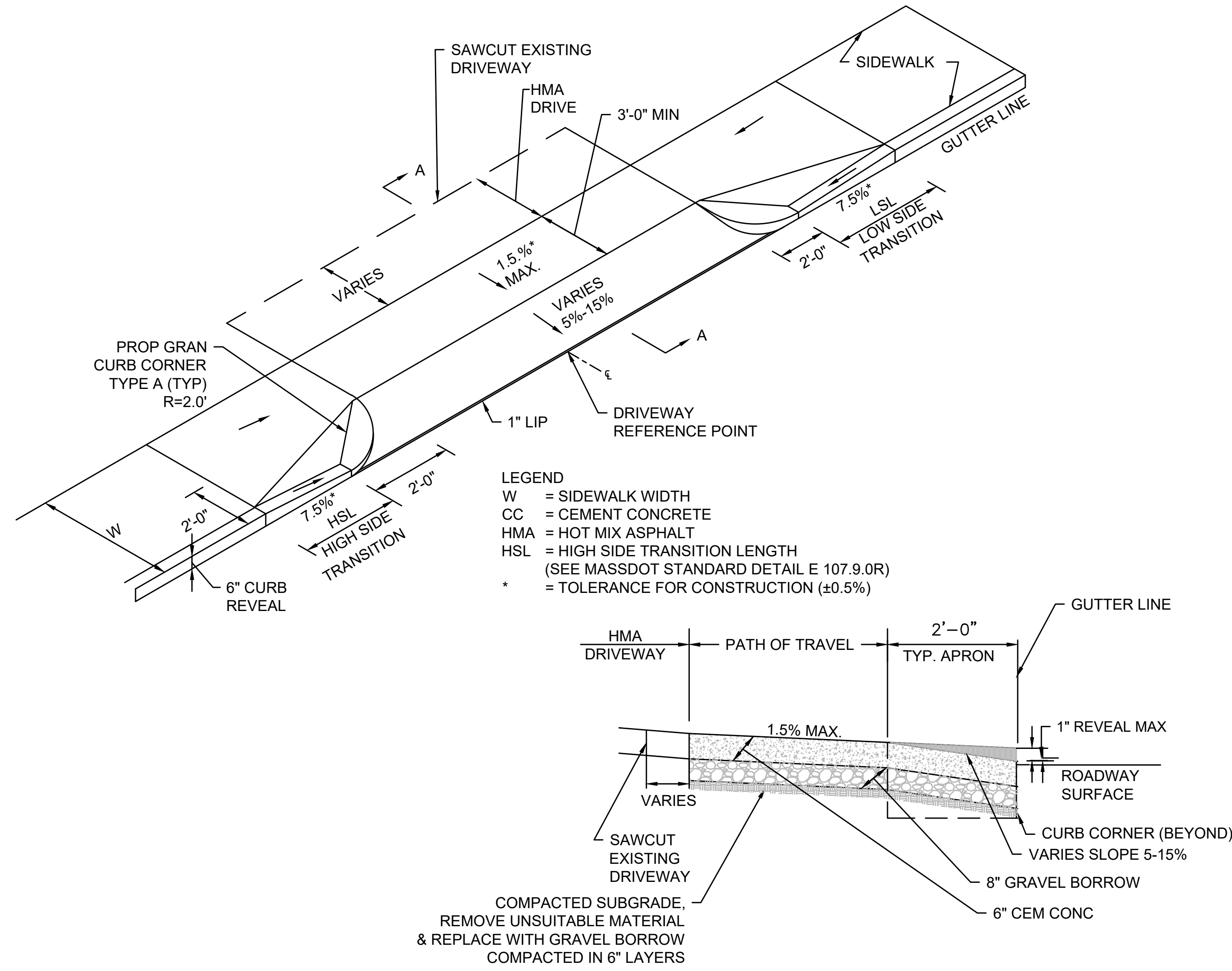
TWO PEDESTRIAN CURB RAMPS WITH
 ONE LEVEL LANDING
 NOT TO SCALE

PEDESTRIAN CURB RAMP DATA										
PCR NUMBER	STREET NAME &	STATION	OFFSET	RAMP WIDTH	SIDEWALK WIDTH	GUTTER SLOPE	LEFT SIDE		RIGHT SIDE	
							CURB REVEAL	CURB TRANSITION LENGTH	CURB REVEAL	CURB TRANSITION LENGTH
8	MELROSE STREET	23+32.07	20.00	8'-0"	5'-0"	0.82%	0'-6"	N/A	0'-6"	N/A
9	LYNN FELLS PARKWAY	5+55.00	22.00	5'-0"	10'-0"	2.11%	0'-6"	N/A	0'-6"	N/A

**COMMONWEALTH OF MASSACHUSETTS
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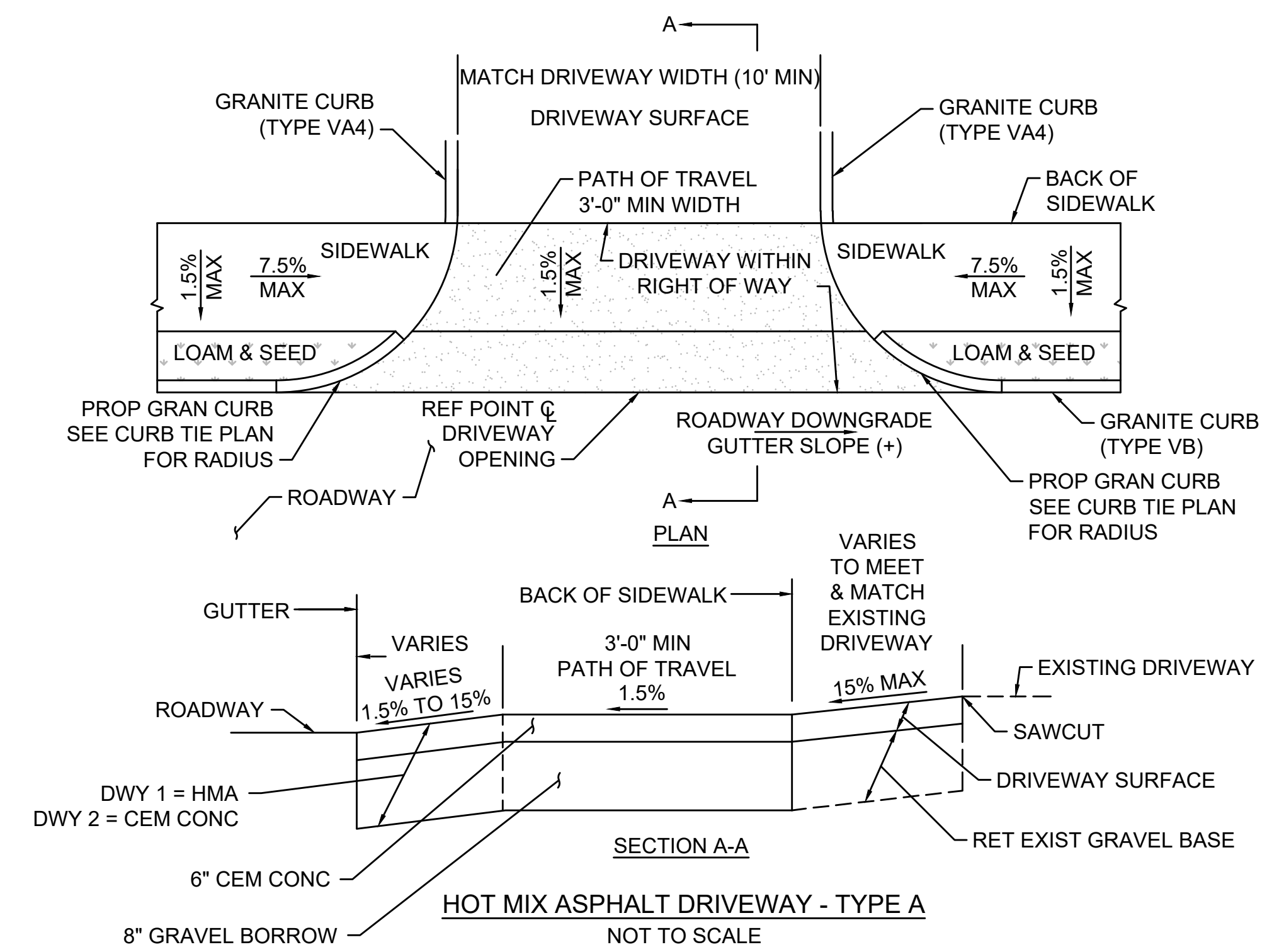
INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

SURVEY BY: NITSCH	CONSTRUCTION DETAILS	SHEET NO.	
DRAWN BY: KC		32	
CHECKED BY: BG		CONT: P20-3361-D1A	SCALE: AS NOTED
APPROVED BY: JM		DATE: 4/11/2023	32 OF 33



**SIDEWALK THROUGH DRIVEWAY
 WITH CURB RETURNS
 2' CURB CORNER
 NOT TO SCALE**

DRIVEWAY DATA - SIDEWALK WITHOUT CURB RETURNS							
DRIVEWAY NUMBER	STREET NAME &	STATION	OFFSET	SIDEWALK WIDTH	ROADWAY GUTTER SLOPE	CURB TRANSITION LENGTH	
						LEFT SIDE	RIGHT SIDE
3	LYNN FELLS PARKWAY	6+73.26	22.00	10'-0"	2.85%	6'-6"	11'-0"
4	LYNN FELLS PARKWAY	7+16.73	22.00	10'-0"	3.17%	6'-6"	14'-0"



DRIVEWAY DATA - HOT MIX ASPHALT DRIVEWAY TYPE A					
DRIVEWAY NUMBER	STREET NAME &	STATION	OFFSET	SIDEWALK WIDTH	ROADWAY GUTTER SLOPE
1	LYNN FELLS PARKWAY	5+01.95	29.24	10'-0"	2.11%
2	LYNN FELLS PARKWAY	5+50.00	22.00	10'-0"	2.11%

**COMMONWEALTH OF MASSACHUSETTS
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 DIVISION OF ENGINEERING AND
 DIVISION OF PLANNING AND RESOURCE PROTECTION**

INTERSECTION IMPROVEMENTS AT
 LYNN FELLS PARKWAY AND MELROSE STREET
 MELROSE

SURVEY BY: NITSCH DRAWN BY: KC CHECKED BY: BG APPROVED BY: JM	CONSTRUCTION DETAILS
CONT: P20-3361-D1A SCALE: AS NOTED DATE: 4/11/2023	SHEET NO. 33 33 OF 33