



# City of Melrose

COMMUNITY MITIGATION FUND  
PUBLIC INPUT SESSION

AUGUST 19, 2020

# What is the Community Mitigation Fund?



- Grant funding from the Massachusetts Gaming Commission
- Provided as mitigation for Encore casino in Everett
- Melrose received \$100,000
- Must be tied to mitigation for casino impacts

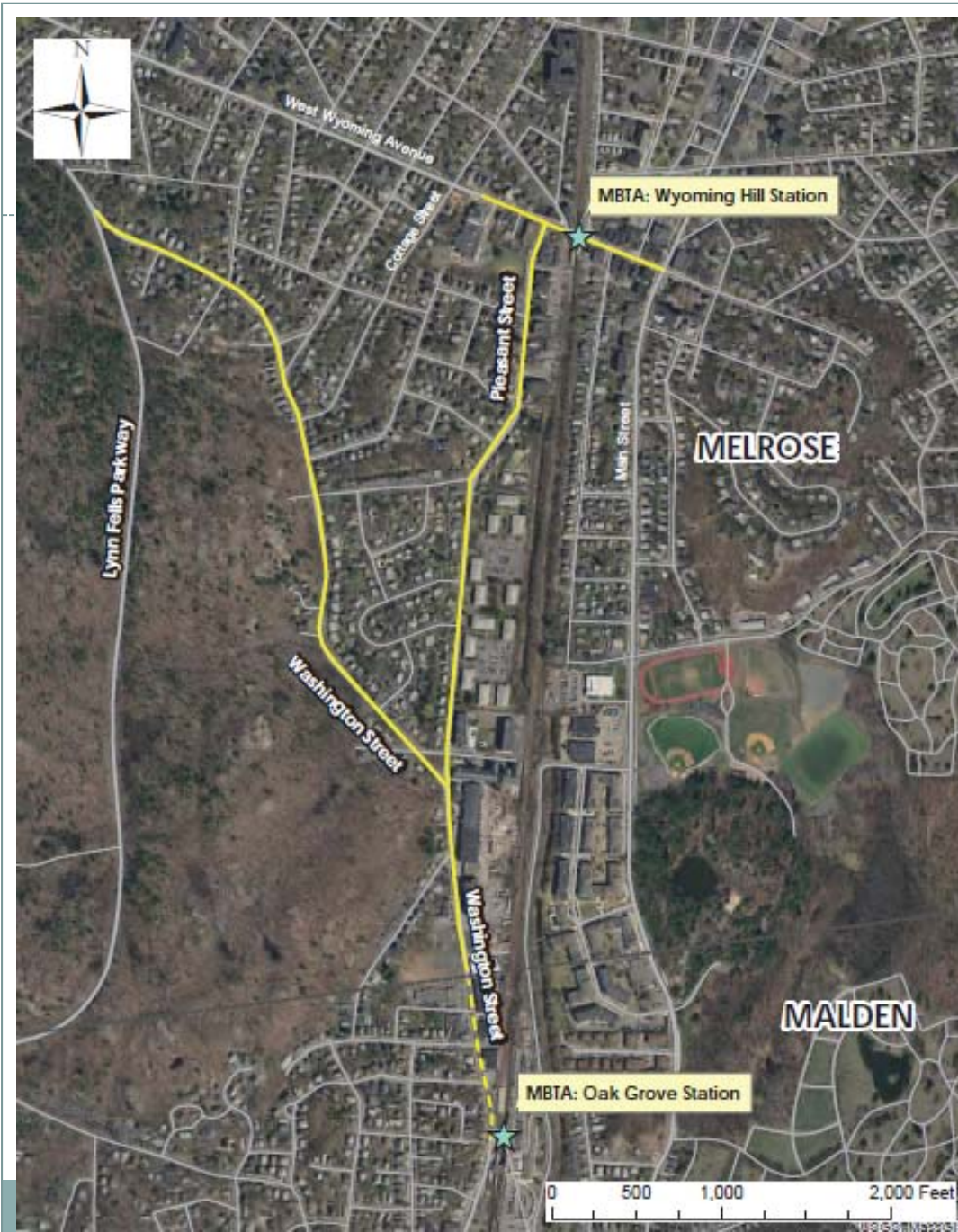
# Project Steps



- Contract signed with Gaming Commission in 2018 for \$100,000
- First \$26,904 for the following:  
*“Develop an engineering planning study based on Complete Streets to provide a Corridor Mitigation Assessment. The planning study is to identify and quantify infrastructure improvements along the Washington Street, Pleasant Street and West Wyoming Avenue corridors.”*
- Remainder will be to implement selected recommendations from the study



- Melrose's eligibility based on Route 99 connection
- Project area selected due to proximity to Oak Grove
- Casino runs shuttle services from the Orange Line



- City contracted with Stantec Engineers to perform assessment
- Focused on pedestrian, bicycle, and accessibility accommodations
  - Washington Street
  - Pleasant Street
  - West Wyoming from Cottage to Main

# Corridor Assessment



- Roadway widths
- Sidewalk widths
- Sidewalk ADA compliance
- Sidewalk conditions
- Curb ramp ADA compliance
- Line painting/crosswalks
- Bus stop ADA compliance
- Signage





# Deficiencies Identified – Washington Street



- Travel lanes wider than necessary
- Speeding
- No room for cyclists
- Some sidewalks in poor condition
- Lack of accessible curb ramps
- Lack of sidewalks on most of southbound side
- Lack of on-street parking
- Difficult intersection at Fellsway East (has been more recently improved)



# Deficiencies Identified – Pleasant Street



- Difficult sight lines
- Lack of crosswalk signage
- Bus stop with poor sight lines
- Some sidewalks in poor condition
- Lack of accessible curb ramps
- Lack of crosswalks across Pleasant Street between Wyoming and Stone Place

# Deficiencies Identified – W. Wyoming Ave.



- Some sidewalks in poor condition
- Bus stop at Cottage Street lacks any accessibility accommodations
- Challenges with Lincoln School arrival/dismissal

# Stantec Recommendations – Washington Street



- Reduce striped travel lanes to 11 feet wide with 3 foot shoulders (slow speeds and add improved area for cyclists)
- Add “chevron” warning signs on curves
- Add digital speed feedback sign
- Replace damaged sidewalk panels
- Reconstruct all curb ramps for ADA compliance

# Stantec Recommendations – Washington Street



- Add curb ramps at Shadow Street
- Create on-street parking with minor roadway widening between Shadow and Crescent
- Consider bicycle accommodations between Pleasant Street and Malden line
- Realign intersection at Fellsway East (DCR property and grades may not work) and improve signage/lighting

# Stantec Recommendations – Pleasant Street



- Stripe crosswalks at crossings parallel to Pleasant Street
- Reduce width of side street openings onto Pleasant Street to reduce crossing distances
- Install new crosswalks across Pleasant Street between Wyoming and Stone Place, including near bus stops
- Replace damaged sidewalk panels and use asphalt where roots create issues
- Relocate bus stop at Pleasant/Gould for safety
- Consolidate MBTA bus stops if possible
- Consider striping for 11-foot lanes and wider shoulders

# Stantec Recommendations – West Wyoming



- Reconstruct sidewalks and curb ramps, including ensuring proper sidewalk widths
- Redesign bus stop at Cottage Street for ADA compliance
- Improve area of drop-offs/pick-ups at Lincoln School
- Consider striping for 11-foot travel lanes

# City Recommendations



- Paint all missing crosswalk markings at cross streets (crosswalks that are parallel with the main roads, some of which are missing)
- Make the bus stop at Wyoming and Cottage accessible; presently bus riders exit onto a grass strip with curbing, rather than onto a sidewalk
- Repaint Washington Street to have 11-foot travel lanes and a 3-foot shoulder and paint bicycle sharrows (there is not enough room for bike lanes anywhere within the corridor)
- Repaint Pleasant Street to have 11-foot travel lanes and shoulders with varying widths as allowable.

# City Recommendations



- Ensure that there is a consistent corridor of sidewalks and ramps on at least one side of each road
- Evaluate whether there is a safe location (or two) to add curb ramps and crosswalks across Pleasant Street between Wyoming and Stone Place (the north entrance to Converse Lane has been identified as one safe location)
- Consider relocating crosswalk across Washington Street from Trenton Street to Clinton Road (better sight lines)
- Consider adding a crosswalk at Pebble Road
- Add a pushbutton rapid flashing beacon to the crosswalk across Washington Street near #99



# Public Input



- Note that all of these improvements cannot be accomplished with the \$74K remaining
- What are your top priority improvements from those listed?
- Are there improvements you think we missed that relate to pedestrian, bicycle, and accessibility improvements in this corridor?
- Other comments?

# Thank You for Participating!

